



## PLANNING COMMISSION MEETING

## STAFF REPORT

**Thursday, September 29, 2016**

Item 1  
PL2016-147  
6:00 p.m.

Peace Lutheran Church  
8600 East Bush Lake Road; 7101 and 7103 West 86<sup>th</sup> Street  
Variance to reduce the side yard setback for building addition from 60 feet to 35; Conditional Use Permit for a place of assembly expansion and Final Site and Building Plans for a 2,950 square foot building addition and parking lot improvements

Item 2  
PL2016-146  
6:05 p.m.

Bloomington Central Station LLC  
8170 31st Avenue, 8170 33rd Avenue, 8151 30th Avenue, and 3098 East Old Shakopee Road  
Preliminary and Final Plat for Bloomington Central Station 5th Addition; Major Revision to Final Development Plan for a 1,666 stall parking structure and surface parking improvements

Originator  
Community Development

Item  
**Variance, CUP Amendment, FSBP**

Date  
9/29/2016

Description

## GENERAL INFORMATION

Applicant: Peace Evangelical Lutheran Church  
Location: 8600 East Bush Lake Road  
  
Request: Variance to reduce the side yard setback for Building Addition from 60 feet to 35 feet; Conditional Use Permit for a place of assembly expansion and Final Site and Building Plans for an approximately 2,950 sq. ft. Building Addition and Parking Lot Improvements.

## AGENCY ACTION DEADLINE AND PUBLIC NOTIFICATION

Application Date: 08/24/16  
60 Days: 10/23/16  
120 Days: 12/22/16  
**Applicable Deadline: 10/23/16**  
Newspaper Notification: Confirmed – (09/15/16 Sun Current – 10 day notice required)  
Direct Mail Notification: Confirmed – (500 foot buffer – 10 day notice required)

## STAFF CONTACT

Nick M. Johnson, Planner  
(952) 563-8925 – [nmjohnson@BloomingtonMN.gov](mailto:nmjohnson@BloomingtonMN.gov)

Requested Action

Staff recommends approval of the variance request to reduce the front yard setback from 60 feet to 35 feet for an addition to a place of assembly. In addition, Staff recommends approval of a Conditional Use Permit amendment for the expansion of a place of assembly, and Final Site and Building Plans for a place of assembly addition and parking lot improvements. Staff recommends the following motions:

In Case PL2016-147, having been able to make the required findings, I move to recommend approval of a variance to reduce the front yard setback of a place of assembly addition at 8600 East Bush Lake Road from 60 feet to 35 feet, subject to the conditions and Code requirements attached to the staff report.

In Case PL2016-147, having been able to make the required findings, I move to recommend approval of a Conditional Use Permit amendment and Final Site and Building Plans for a place of assembly addition and parking lot improvements at 8600 East Bush Lake Road, subject to the conditions and Code requirements attached to the staff report.

---

Attachments:

Staff Report  
Notification Map  
Affidavit of Publication  
Project Description  
Plans  
DRC Minutes  
DRC Comment Summary

## GENERAL INFORMATION

Applicant: Peace Evangelical Lutheran Church

Location: 8600 East Bush Lake Road

Request: Variance to reduce the side yard setback for Building Addition from 60 feet to 35 feet; Conditional Use Permit for a place of assembly expansion and Final Site and Building Plans for an approximately 2,950 sq. ft. Building Addition and Parking Lot Improvements.

Existing Land Use and Zoning: Place of Assembly; zoned Single Family Residential (R-1)

Surrounding Land Use and Zoning: North, South and East – Park; zoned SC  
West – Single Family Residential and pond; zoned R-1 and Conservation

Comprehensive Plan Designation: Quasi-Public, Low Density Residential and Conservation

## HISTORY

City Council Action: 06/02/80 – Approved a Conditional Use Permit for a place of assembly (Case #08703D-80) and variances to reduce the principal structure setbacks. (Case #8703ABCE-80)

## CHRONOLOGY

Planning Commission                      09/29/2016      Public Hearing Scheduled

## PROPOSAL

Peace Lutheran Church is requesting a variance to reduce the setback along East Bush Lake Road from 60 feet to 35 feet in order to construct an approximately 2,950 square foot addition to an existing place of assembly located at 8600 East Bush Lake Road. The request includes Final Site and Building Plans and an amendment to an existing Conditional Use Permit for a place of assembly expansion in the R-1 Single Family Residential zoning district. The proposed building addition includes new administrative offices, a commercial kitchen, bathroom facilities, a trash and recycling room and additional storage space. Moving these uses to new space within the proposed addition would allow for the conversion of existing space to four new classrooms on the main floor.



The place of assembly is also proposing improvements to their existing parking lot, as well as constructing a new parking lot on the site of an existing two-family dwelling located at 7103 and 7101 West 86<sup>th</sup> Street. The two-family dwelling would be removed to make way for the new parking area west of the existing parking lot.

As part of the Conditional Use Permit amendment, Peace Lutheran Church requests the City consider a non-concurrent use condition for parking. The purpose of the non-concurrent use condition is ensure the site will be in conformance with the City's parking regulations (Sec. 21.301.06 of the City Code). The City amended its parking quantity requirements since the place of assembly was expanded/constructed in 1980. The existing parking provided is legally non-conforming. Place of assembly expansion as proposed brings the overall level of code-compliance into review via the Conditional Use Permit amendment process. The non-concurrent use condition, along with the construction of additional parking, would be to bring the site into conformance with parking regulations. More specifically, the agreement would restrict Peace Lutheran Church's ability to utilize all assembly and programming spaces within the facility at the same time, thereby preventing potential scenarios of inadequate parking on-site. Should the non-concurrent use condition be approved, the restrictions would be memorialized as a part of the Conditional Use Permit amendment.

## **ANALYSIS**

The review of the proposed building addition, parking lot improvements and non-concurrent use condition are based on the performance standards applicable to places of assembly and non-residential structures located in the R-1 Single Family Residential zoning district.

### **Land Use**

To accommodate the proposed building addition and parking lot improvements, Peace Lutheran Church is requesting to amend their Conditional Use Permit (CUP) for a place of assembly in the R-1 zoning district. A place of assembly is a conditional use in the R-1 Single Family Zoning District. Peace Lutheran Church received approval for their existing CUP in 1980 prior to the construction/expansion of the place of assembly. Expansion of Peace Lutheran Church requires the existing CUP to be amended to document the building addition, parking improvements and the proposed non-concurrent use condition.

### **Code Compliance**

Table 1 includes applicable performance standards related to the proposed building addition and parking lot improvements in the R-1 zoning district. In addition, the building addition and site improvements are evaluated against performance standards for places of assembly.

**Table 1: City Code Analysis for Places of Assembly in the R-1 Zoning District**

| Standard and Code Section   | Proposed   | Meets Standard?  |
|---|--|--|
| <b>Building Setbacks</b><br>(Sections 21.301.02(d)(2) and 19.63(a)(4):<br>Front-yard setback – 60 feet<br>Side-yard setback – 25 feet<br>Rear-yard setback – 30 feet                | Setbacks (smallest proposed):<br>Front-yard (north and east) – 35 feet<br>Side-yard (west) – no change<br>Rear-yard (north) – no change  | Variance requested for reduced setback to East Bush Lake Road (see comments) |
| <b>Maximum Structure Lot Coverage – 15 %</b> (Section 19.63(c)(1))  | 8.3%   | Yes  |
| <b>Maximum Parking Lot Coverage – 70%</b> (Section 19.63(c)(2))   | 35.5%<br>37.4% w/proof of parking  | Yes  |
| <b>Parking Location</b> – No parking shall be permitted on the site in front of the place of assembly except for existing sites with previously approved CUP (Section 19.63(a)(9)). | Existing parking area in front of the structure approved 1980. Northwest parking area is not in front of the structure. A handicap stall in southeast corner is in front of the new building addition. | Revisions required (see comments)  |
| <b>Parking</b> – See Table 2. Total proposed facility would require 174 parking spaces if all portions of the building were used at once.   | 118 parking stalls<br>16 proof of parking stalls<br>134 total parking stalls   | No – Applicant requests a non-concurrent use condition (see comments)        |
| <b>Parking Stall Dimensions</b> – 9 feet by 18 feet for 90 degree parking (Section 21.301.06(c)(2)(C)(i))   | 9 feet by 18 feet  | Yes  |
| <b>Drive Aisles</b> - Adjacent to 90 degree parking must be 24 feet wide. All other drive aisles must be a minimum of 20 feet. Fire Code (Section 21.301.06(c)(2)(C)).              | New parking area – 24 feet<br>Modified existing parking area – 24 feet<br>Eastern existing parking area – 15.2 feet (legally non-conforming)   | Yes  |

| Standard and Code Section  | Proposed   | Meets Standard?                          |
|--|--|--|
| <b>Sidewalk Width</b> - Private sidewalks must have a minimum width of five unobstructed feet (Section 21.301.04(d)(1)(B)).  | All new sidewalks are at least 5 feet in width.  | Yes                                      |
| <b>Landscaping – Trees / Shrubs</b> – One tree per 2,500 sq. ft. and one shrub per 1,000 sq. ft. of disturbed area (Sec 19.52(c)(2)). 57 trees and 143shrubs required. | 92 trees and 206 shrubs  | Yes                                      |
| <b>Landscape Yard</b> –20 feet adjacent to streets and 5 feet for interior lot lines (Section 19.52(c)(4)).  | North – 23 feet<br>West – 5 feet<br>South (new parking) – 5 feet<br>East – 16.6 feet (legally non-conforming)                                    | Yes                                      |
| <b>Parking Island Trees</b> – A minimum of one deciduous tree must be provided per parking lot island (Section 21.301.06(c)(2)(H)(iii).                                | Trees are provided in all new and modified islands except for two islands near entry of new parking lot.   | Revisions required (see comments)        |
| <b>Screening</b> – Parking lots with 6 stalls or more within 30 feet of sites residentially used and zoned must have perimeter screening (Section 19.52(d)(1)(A).      | Perimeter screening is provided in the form of one row of evergreen plantings – one row of evergreens is not Code-compliant – two rows required. | Revisions required (see comments)        |
| <b>Lighting for Parking Surface</b> - 1.0 foot-candle required (Section 21.301.07(c)(13))  | Lighting Plan yet to be submitted  | Photometric Plan required (see comments) |

### **Building Design**

The proposed building addition will extend off the northeast corner of the existing place of assembly. The primary exterior building material would be fiber cement board. The proposed exterior materials are intended to match the existing place of assembly design and are Code-compliant. However, the City Code prohibits the coating of exterior materials for non-residential buildings in the R-1 zoning district. The applicant has been informed of the coating prohibition by City staff.

**Landscaping, Screening and Lighting**

The subject property already has a significant amount of mature landscape material on site. With the addition of new landscape material, the place of assembly is code-compliant with respect to the amount of trees and shrubs proposed. While there is a significant amount of existing material, new landscaping is required to achieve code-compliant parking lot plantings and screening. All new and modified parking islands require a minimum of one deciduous tree. Two of the new parking islands located near the entrance of the new parking lot in the northwest portion of the site are not proposed to have trees. The landscape plan will need to be modified to achieve Code-compliant parking island plantings. It should be noted the boundaries between the place of assembly and two-family dwelling parcels does not currently have a 5-foot landscape yard. Under the provisions of the Zoning Code, the totality of the parcels can be reviewed as one broader zoning lot due to common ownership, thereby nullifying code requirements (setbacks, landscape yards, etc.), if the parcels are combined for tax purposes. Therefore, the 5-foot landscape yard between the place of assembly property and two-family dwelling site is not required if the tax combination is completed. Staff is recommending a condition that the property owner complete a parcel combination for tax purposes of the three subject properties in order for the subject site to be considered a legal zoning lot.

Regarding required screening on site, the new parking lot in the northwest portion of the subject property requires parking lot screening due to proximity to residential sites located to the south and west of the new parking lot. The applicant is proposing to screen the parking lot utilizing one row of evergreen plantings. However, City Code requires that two rows of evergreen plantings be provided in order to be considered an effective screen. Other permitted screening techniques include a berm, an opaque wall/fence, or a combination of the three permitted techniques. The proposed screening proposed along the southern and western property lines of the new parking area will need to be revised in order to comply with City Code. Staff is recommending a condition of approval that Code-compliant screening be provided for the new parking lot.

The applicant has not submitted a photometric lighting plan for the new and existing parking areas. City Code requires 1.0 foot-candle on all parking surfaces. The light level of the perimeter 25 feet of the parking areas can be reduced by 50%, requiring a minimum of 0.5 foot-candles. Staff is recommending a condition that Code-compliant lighting be provided for all parking surfaces of the site. The photometric lighting plan must be approved prior to the issuance of any building permits.

**Access and Circulation:**

The existing driveway to the place of assembly and existing parking lot will remain. However, a landscaped island in the middle of the driveway is proposed to be removed. Other improvements include widening some legally non-conforming drive aisles, making them Code-compliant.

However, the drive aisle, 15.2 feet in width at its narrowest point, on the eastern side of the existing parking lot will remain legally non-conforming, as that portion of the lot will remain undisturbed. Improved pedestrian circulation within the existing parking lot is proposed. The applicant is planning a central pedestrian corridor with sidewalks within planted medians and painted crosswalks to alert drivers to the presence of pedestrians. The central pedestrian corridor will also be connected to the public sidewalk along West 86<sup>th</sup> Street, better connecting the place of assembly to the minor collector roadway. These improvements should provide safer and more efficient pedestrian travel through the site.

The new proposed parking lot on the two-family dwelling site northwest of the place of assembly would also be accessed via a driveway from West 86<sup>th</sup> Street. The driveway for the new parking lot is approximately 175 feet west of the driveway of the place of assembly site. Once vehicles park in the overflow lot, pedestrian connection to the place of assembly site is provided via a sidewalk and staircase in the southeast corner of the new parking lot. As described above, the properties are separate parcels of record, but for purposes of setbacks, they are considered one zoning lot if a tax record combination of the subject parcels is completed. However, there is an existing public drainage and utility easement on the lot line that needs to be handled. The applicant may either apply to vacate the easement or apply for an encroachment agreement with the City.

### **Parking**

The subject property currently has 92 parking stalls, many of which are not Code-compliant from a dimensional and drive aisle standpoint. As the place of assembly was legally constructed and expanded prior to current parking regulations being adopted, the property is legally non-conforming with regards to the quantity of parking stalls. According to the applicant, the existing parking facilities can accommodate 96% of the services and events that are hosted at Peace Lutheran Church. The other 4% of services/activities, including Christmas Eve, Easter and well attended funerals, occasionally result in overflow parking occurring on West 86<sup>th</sup> Street or other surrounding residential streets. As part of the building addition project, the applicant intends to add parking to reduce the potential for overflow on-street parking occurring during the limited number of large events. To achieve this goal, Peace Lutheran Church is proposing to remove the existing two-family dwelling on the site to the northwest of the place of assembly to construct a new parking lot (45 stalls and 9 proof of parking stalls).

Through the modification of the existing parking lot and the construction of the new northwest parking lot, Peace Lutheran Church would have 118 code-compliant parking stalls, representing an increase of 26 parking stalls. In addition, the place of assembly is proposing a design that would allow 16 additional stalls to be constructed as part of a proof of parking agreement, bringing the potential total of parking to 134 legal stalls on-site. Given that the proposed

addition is 2,950 square feet in size and would require 5 additional parking stalls (primarily office and kitchen space) to be added by Code, the proposed increase in parking above and beyond what Code would require for the addition would be 21 parking spaces (26 spaces added – 5 spaces required = 21 excess spaces). However, the fact that places of assembly are conditional uses in the R-1 zoning district requires that the totality of the site be reviewed for Code-compliance. Table 2 provides a parking analysis of the total facility with the proposed building addition included.

**Table 2: Parking Analysis for Peace Lutheran Church w/Proposed Addition**

| Use Description                          | Standard  | Proposed<br>(square feet)    | Required Parking<br>(stalls) |
|--|---|------------------------------|------------------------------|
| Offices                                  | 1 stall per 285 gross square feet   | 1,021                        | 3.6                          |
| Classrooms                               | 1 stall per 800 gross square feet   | 3,485                        | 4.4                          |
| Nursery                                  | 1 stall per 1/3 occupancy of 1 person per 120 square feet                         | 503                          | 1.4                          |
| Kitchen                                  | 1 stall per 1/3 occupancy of 1 person per 100 square feet                         | 425                          | 1.4                          |
| Sanctuary                                | 1 stall per 1/3 occupancy of 1 person per 15 square feet and/or sanctuary seating | 217 square feet<br>264 seats | 92.8                         |
| Narthex<br>(South and Mid)               | 1 stall per 1/3 occupancy of 1 person per 15 square feet                          | 1,344                        | 29.9                         |
| Parish Hall                              | 1 stall per 1/3 occupancy of 1 person per 15 square feet                          | 1,685                        | 37.4                         |
| <b>Total Parking Requirement</b>         |   |                              | <b>170 stalls</b>            |
| <b>Parking Proposed</b>                  |   |                              | <b>118 stalls</b>            |
| <b>Proof of Parking Proposed</b>         |   |                              | <b>16 stalls</b>             |
| <b>Total Parking Proposed with Proof</b> |   |                              | <b>134 stalls</b>            |

With the proposed building addition, a total of 170 parking stalls would be required to serve the totality of the place of assembly facility according to current parking regulations. Given its historic use characteristics, the place of assembly requests that the City consider a formal non-concurrent use condition. The condition would formally limit which portions of the place of assembly facility could be used at which time, regulating and minimizing overall parking demand. Additional details and analysis of the proposed non-concurrent use condition are presented in the section that follows. Non-concurrent use conditions have been applied at multiple place of assembly and school sites in Bloomington. Such conditions avoid the construction of parking that rarely, if ever, will be used.

Proof of parking is a flexibility measure utilized for properties that have a clear or verified parking demand below the Code requirement. This requires an agreement be executed by both the property owner and the City and recorded against the property. Should the additional parking stalls be needed to serve additional parking demand, the City can require the property owner to install the proof of parking stalls to provide additional parking facilities.

In this case, the applicant requests 16 proof of parking stalls. Given that the nature of parking demand at places of assembly is typically defined by moderate use the vast majority of the time and peak parking demand only a few occurrences a year, staff supports proof of parking approach in this case. If the parking facilities provided prove not to be sufficient to serve the place of assembly, the City can require the place of assembly to install 16 additional parking stalls to meet the increase demand. Staff is recommending a condition of approval that the applicant enter into a proof of parking agreement to memorialize the proposed proof stalls.

Regarding the current parking lot design, there is one space that currently does not comply with City Code. The performance standards for places of assembly (Sec. 19.63. (a)(9)) do not allow parking to be located between the street and the building unless previously approved by a Conditional Use Permit (CUP). All of the parking spaces in the existing parking lot meet the exemption, as they were previously approved through the CUP in 1980. However, one handicapped parking stall proposed in the southeast corner of the existing parking lot now would be located in between the place of assembly and East Bush Lake Road as a result of the building addition extending further north. Consequently this stall as proposed does not meet the parking performance standard for places of assembly. This stall will need to be removed in order for the parking design to be fully Code-compliant. Staff is recommending a condition that this non-conformity be corrected by removing the parking stall from this location.

To provide a general summary of the overall parking analysis, staff would like to highlight the following:

- There are 92 existing parking spaces at Peace Lutheran Church, several of which are legally non-conforming.
- The place of assembly facility is currently legally non-conforming with regards to parking quantity.

- The applicant is proposing 118 parking spaces (26 space increase over existing condition) and 16 proof of parking spaces.
- The proposed place of assembly addition would require 5 additional parking spaces per Code.
- With the proof of parking stalls, the total number of spaces would be 134. For the place of assembly to conform to the required level of total parking, the site would need 170 parking spaces. Therefore the site would be 36 spaces short of the Code-compliant amount of parking with the addition of the proof stalls.
- The place of assembly is proposing a non-concurrent use condition to address the parking shortfall as determined by current parking regulations.

Given the proposed place of assembly expansion is 2,950 square feet (5 additional spaces required), and a net 26 parking stalls are added to the property, staff is supportive of the building expansion and CUP amendment provided a non-concurrent use condition that further regulates parking demand is executed.

### **Non-Concurrent Use Condition**

Non-concurrent use conditions have been frequently used at Bloomington place of assembly and school sites to avoid the construction of unnecessary parking. As it is atypical or uncommon for all assembly spaces within places of assembly to be utilized or occupied all at the same time, non-concurrent use conditions formally restrict use with the goal of properly managing parking demand. The benefit of such a condition is to not overbuild parking facilities that will only be used for rare or infrequent events. In this case, Peace Lutheran Church is proposing to abide by a non-concurrent use condition with the City in order to manage parking demand and demonstrate compliance with the City's parking requirements to the greatest extent possible. Table 3 presents the non-concurrent use scenarios the Peace Lutheran Church is proposing.

**Table 3: Non-Concurrent Use Scenarios for Peace Lutheran Church**

| Use Scenario   | Facilities Used<br>(Required Parking Spaces)   | Total Parking Requirement |
|--|--|---------------------------|
| <b>Scenario #1</b><br>(Worship Use - Regular)                      | Classrooms (4.4), nursery (1.4), offices (3.6), kitchen (1.4) and sanctuary (92.8)   | 104                       |
| <b>Scenario #2</b><br>(Worship Use – Christmas and Easter)         | Nursery (1.4), sanctuary (92.8) and narthex (29.9)   | 124                       |
| <b>Scenario #3</b><br>(Visitation or Wedding – Non-Ceremony Space) | Classrooms (includes Lounge) –three used for assembly (18.2), nursery (1.4), offices (3.6), kitchen (1.4), narthex (29.9) and parish hall (37.4) | 92                        |



As demonstrated in Table 3, the proposed use scenarios generate parking requirements below or proximate to the quantity of parking stalls installed, which is 118 parking spaces. When factoring in the possibility of installing proof of parking stalls, the total amount of parking at the place of assembly can be increased to 134 parking spaces.

Only the parking requirement of use scenario #2 (Christmas, Easter or large funeral), which only occurs a few times per year, exceeds the amount of parking proposed to be installed as part of the subject project. Through the use of a proof of parking agreement, the City will have the ability to monitor the parking situation at Peace Lutheran Church to determine if any additional parking spaces are necessary. Once again, if peak parking demand only occurs 4-6 times per year, which is common for places of assembly, the installation of additional parking to meet infrequent peak demand is not the recommended solution. If demand rises to a level that creates a public safety problem on adjacent roadways, the City can then execute against the proof of parking agreement to require additional stalls (up to 16 as proposed) to be installed. Given the nature of use for places of assembly, a non-concurrent use condition can be an effective tool to manage parking demand. Staff is supportive of such a condition in this case and is recommending that the City adopt non-concurrent use conditions. More specifically, staff finds that the non-concurrent use scenarios described by the applicant can be achieved if there is no concurrent use of the Sanctuary and Parish Hall. Staff is recommending a condition that these two facilities are not used concurrently.

### **Variance Request**

As part of the request for the building addition, Peace Lutheran Church is requesting a variance for a reduced building setback from 60 feet to 35 feet along East Bush Lake Road. When the place of assembly facility was constructed/expanded in 1980, a similar variance was approved, allowing for a setback reduced to 30 feet. In examining the survey of the property, the place of assembly was constructed 34.5 feet from the East Bush Lake Road right-of-way. It is the applicant's intention that the proposed building addition to the northeast corner of the structure would follow a similar setback and building line that the previous place of assembly construction/expansion followed.

In requesting a reduced setback along East Bush Lake Road, the applicant notes three factors that inform the variance request:

- 1) Approval of the variance would allow for the building addition to follow the same setback and building line that was previously approved;
- 2) The irregular shape of the subject property limits the place of assembly's ability to meet the required setbacks from East Bush Lake Road, representing a unique circumstance not created by the landowner; and
- 3) East Bush Lake Road is a collector road, necessitating a larger right-of-way and thereby further constraining the site.

Staff has reviewed the reasons presented by the applicant in requesting the variance and found them to be accurate. Based on the proposed building addition following the existing building line and setback that was established by a previous variance and the irregular shape of the subject property, the variance request has merit and staff recommends approval. Additional analysis of the variance request in the form of proposed findings is provided in the Findings section of the staff report.

**Stormwater Management:**

Stormwater must be managed to meet the City's and Watershed District's requirements for stormwater rate control (quantity), stormwater quality, and volume. Peace Lutheran Church is proposing to install an underground infiltration and storage basin in the western portion of the existing parking lot. The applicant has not yet provided a stormwater management plan with calculations.

A maintenance plan has not yet been provided and will be required to be signed and filed at Hennepin County. Prior to the issuance of any building or grading permits, the stormwater management plan must be approved by the City Engineer. This site is located within the Nine Mile Creek Watershed District, so an additional permit will be required.

**Utilities:**

There is an existing 16-inch watermain located under W. 86<sup>th</sup> Street, and the place of assembly already has a 6-inch water service. The applicant proposes installing a new gate valve in the 16-inch watermain and then tapping another 6-inch water service just west of the existing service. This will provide a watermain loop for the site. An additional hydrant is proposed to be added to a parking island near the front of the building. This hydrant needs to be within 50-feet of the Fire Department Connection. No changes are proposed to the sanitary sewer service.

**Transportation Demand Management (TDM):**

This redevelopment will require a Tier 2 TDM plan, which allows the property owner to choose from a menu of TDM options. The owner has not yet submitted a Tier 2 TDM checklist. In addition, the applicant should plan on installing bike racks, as approved by the City Engineer.

**Fire Preventions and Public Safety**

Maintaining access, water supply, and addressing for the property are critical factors as the proposed development will be comprised of a place of assembly, classrooms and nursery. The current parking lot for the place of assembly doesn't meet the required code complying access and circulation for emergency vehicle access. Any restructuring of the undisturbed portion of

the existing parking area will require all emergency vehicle access and turning radii to be compliant with adopted code requirements.

The hydrant must be maintained and/or relocated for the existing building. A single, combined water service into the building is required for the domestic and sprinkler system water supply. The fire alarm system shall meet the minimum requirements for an assembly occupancy.

The buildings must be addressed plainly and visibly from the street or road using numbers that contrast with the background. The numbers must be a minimum of four inches and be Arabic numbers or alphabetic letters.

Any changes made to the current plans, including building location, access roads, water supply and addressing shall be reviewed by the Fire Marshal to ensure continued compliance with the fire code. The proposed changes to the existing building will require the place of assembly to be fully sprinkled.

#### **Status of Enforcement Orders**

There are no open enforcement orders for this property.

#### **FINDINGS**

##### **Variance Findings – Section 2.98.01 (b)(2)(A-C)**

##### **A) That the variance is in harmony with the general purposes and intent of the ordinance;**

- A place of assembly is a conditional use in the R-1 Single Family Residential Zoning District. The requested variance would allow for a 2,950 square foot expansion of Peace Lutheran Church to add new bathroom, kitchen, storage and office facilities. These uses are consistent with the typical support functions of a place of assembly. In addition, the proposed location of the place of assembly addition still allows for a significant setback to East Bush Lake Road due to the increased size of the right-of-way and boulevard for the collector roadway. The requested encroachment of the place of assembly addition into the required setback is also consistent with a previously approved setback variance for the place of assembly. The variance is in harmony with the general purposes and intent of the zoning ordinance.

##### **B) That the variance is consistent with the comprehensive plan;**

- The subject property is guided Quasi-Public, Low Density and Conservation with the addition taking place in the Quasi-Public designation. Places of assembly are consistent with the land use designation Quasi-Public. The expansion of a place of assembly at 8600 East Bush Lake Road is consistent with the Comprehensive Plan.

**C) When the applicant for the variance establishes that there are practical difficulties in complying with the zoning ordinance.**

- The applicant's practical difficulty is created by the irregular shape of the subject property, thereby constraining Peace Lutheran Church's opportunity for facility expansion. The proposed encroachment into the front yard setback is less than the previous encroachment of the principal building as approved by a variance in 1980 at the time of place of assembly construction.

**Practical difficulties as used in connection with the granting of the variance, means that:**

**(i) The property owner proposes to use the property in a reasonable manner not permitted by the zoning ordinance;**

- Peace Lutheran Church proposed a 2,950 square-foot expansion to add a kitchen and office space to an existing place of assembly that was constructed in 1980. Due to the irregular lot shape, the place of assembly previously received a setback variance to reduce the setback to East Bush Lake Road from 60 feet to 30 feet when it was originally expanded. The proposed expansion of the place of assembly necessitates a similar setback variance, reducing the setback from 60 feet to 35 feet. The proposed expansion of the place of assembly is a reasonable use not permitted by an official control.

**(ii) The plight of the landowner is due to circumstances unique to the property not created by the landowner; and**

- The subject property is irregularly pie-shaped, as the eastern property line runs southwest-northeast. As a result, the central and southern portion of the property becomes more and more constrained from north to south, leaving limited area for a building footprint. Given the constrained building area created by the irregular lot shape, it is difficult to meet the required 60-foot setback to all public street frontages for places of assembly. In addition to the challenge caused by the lot shape, it should also be noted that the boulevard for East Bush Lake Road is approximately 30 feet in size. While East Bush Lake Road is high-volume, major collector road, a 30-foot boulevard still

provides significant separation and setback for the proposed addition. As proposed, the place of assembly addition would be 65 feet from the edge of roadway for East Bush Lake Road. Both the irregular lot shape and significant boulevard of East Bush Lake Road are unique circumstances not created by the landowner.

**(ii) The variance if granted will not alter the essential character of the locality.**

- Peace Lutheran Church has been located at 8600 East Bush Lake Road since 1980. The proposed place of assembly addition is 2,950 square feet in size. Based on the existing area of the place of assembly, 11,957 square feet, the proposed expansion represents a 24.6% increase in floor area. In addition, the proposed addition will not allow for a significant increase of assembly or congregation space. The proposed addition to the place of assembly is not considered significant with respect to increased size or intensity of use. Finally, the place of assembly previously received a variance for a reduced setback of 30 feet to East Bush Lake Road. The proposed addition will be setback equivalent to the existing condition. The requested variance will not alter the essential character of the locality.

**Section 21.501.04(e)(1-5) - Conditional Use Permits**

**Section 21.501.01(d)(1-4) – Final Site and Building Plans**

**(1) The proposed use/development is not in conflict with the Comprehensive Plan;**

- The subject property is guided Quasi-Public, Low Density residential and Conservation in the Comprehensive Plan. An addition to an existing place of assembly on a parcel with the Quasi-Public land use designation does not conflict with the Comprehensive Plan.

**(2) The proposed use/development is not in conflict with any adopted District Plan for the area;**

- The property is not located within an area with an adopted District Plan.

**(3) The proposed use/development is not in conflict with City Code provisions;**

- Subject to compliance with the conditions of approval and approval of a requested setback variance, the proposed use/development meets all City Code requirements.

**(4) The proposed use will not create an excessive burden on parks, schools, streets, and other public facilities and utilities which serve or are proposed to serve the planned development; and**

- The proposed use is not of a nature, scale, or intensity to create an excessive burden on parks, schools, or other public facilities and utilities. The proposed place of assembly renovation does not add a significant amount of assembly or programming space. The primary purpose of the addition is to add improved kitchen and bathroom facilities, storage and trash rooms, and office space. All of these uses are typical support functions of a place of assembly and should not increase the intensity of the overall use of the property in a significant way. Finally, the construction of additional on-site parking should reduce the occurrence or need of on-street parking during the facilities largest events, thereby reducing the burden on public facilities such as West 86<sup>th</sup> Street and other adjacent streets.

**(5) The proposed use/development will not be injurious to the surrounding neighborhood or otherwise harm the public health, safety and welfare.**

- The proposed addition to the place of assembly is located on the opposite side of the property from the abutting residential uses. In addition, the proposed expansion should not increase the intensity of use of the existing facility. Finally, the provision of additional parking will reduce the infrequent occurrence of on-street parking being utilized for overflow purposes. The proposed use is not anticipated to be injurious to the surrounding neighborhood or otherwise harm the public health, safety and welfare.

## **RECOMMENDATION**

Staff recommends approval of the variance request to reduce the front yard setback from 60 feet to 35 feet for an addition to a place of assembly. In addition, Staff recommends approval of a Conditional Use Permit amendment for the expansion of a place of assembly, and Final Site and Building Plans for a place of assembly addition and parking lot improvements. Staff recommends the following motions:

In Case PL2016-147, having been able to make the required findings, I move to recommend approval of a variance to reduce the front yard setback of a place of assembly addition at 8600 East Bush Lake Road from 60 feet to 35 feet, subject to the conditions and Code requirements attached to the staff report.

In Case PL2016-147, having been able to make the required findings, I move to recommend approval of a Conditional Use Permit amendment and Final Site and Building Plans for a place of assembly addition and parking lot improvements at 8600 East Bush Lake Road, subject to the conditions and Code requirements attached to the staff report.

## **RECOMMENDED CONDITIONS OF APPROVAL**

**Case** PL201600147

**Project Description:** Variance to reduce the required side yard setback from 60 feet to 35 feet for a building addition; Conditional Use Permit amendment for an addition to a place of assembly; and Final Site and Building Plans for a 2,950 square foot building addition and parking lot improvements.

**Address:** 8600 E BUSH LAKE RD

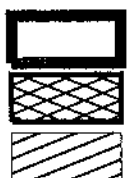
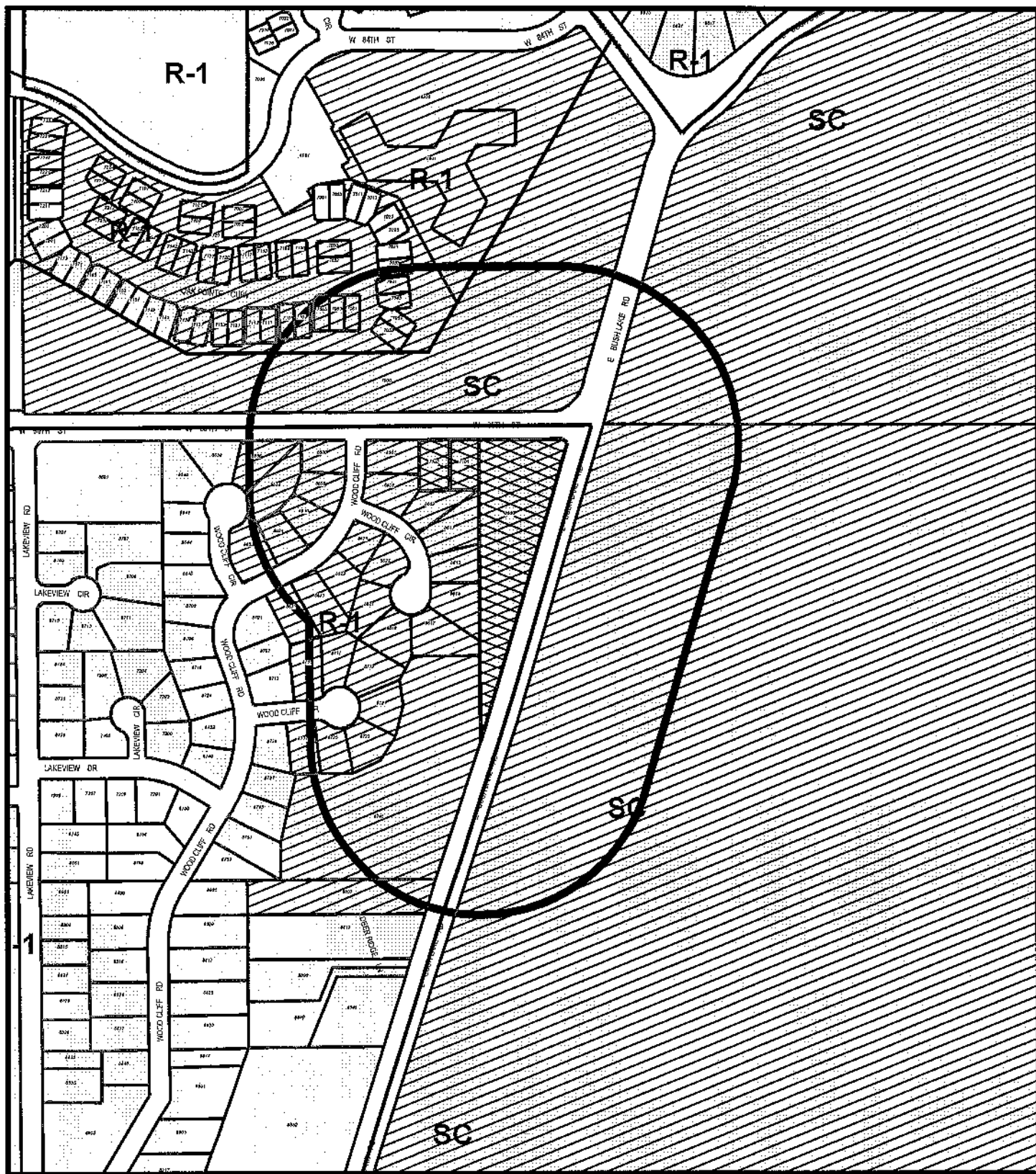
The following conditions of approval are arranged according to when they must be satisfied. In addition to conditions of approval, the use and improvements must also comply with all applicable local, state, and federal codes. Codes to which the applicant should pay particular attention are included below.

1. Prior to Permit A Proof of Parking Agreement for 16 spaces must be approved and filed with Hennepin County.
2. Prior to Permit Sewer Availability Charges (SAC) must be satisfied.
3. Prior to Permit Signed copies of a private common parking/access agreement and a private common utility easement/agreement must be provided to the City and filed with Hennepin County and proof of filing provided to the Engineering Division.
4. Prior to Permit Storm Water Management Plan must be provided that demonstrates compliance with the City's Comprehensive Surface Water Management Plan. A maintenance plan must be signed by the property owners and must be filed of record with Hennepin County.
5. Prior to Permit The Grading, Drainage, Utility, Erosion Control, Access, Circulation, Parking and Traffic plans must be approved by the City Engineer.
6. Prior to Permit A Nine Mile Creek Watershed District permit must be obtained and a copy submitted to the Engineering Division.
7. Prior to Permit A sidewalk must be provided to link the primary entrance of each building on site with the public sidewalk network.
8. Prior to Permit A National Pollutant Discharge Elimination System (NPDES) construction site permit and a Storm Water Pollution Prevention Plan (SWPPP) must be provided if greater than one acre is disturbed (State of MN and Federal regulation).
9. Prior to Permit An erosion control surety must be provided (16.05(b)).
10. Prior to Permit A Minnesota Department of Health (MDH) watermain review and approval must be obtained or notification from MDH that this permit is not required must be submitted to the City (State of MN).
11. Prior to Permit Civil plans must be revised to remove one parking stall located in the southeast portion of the existing parking lot to comply with Section 19.63(a)(9) of the City Code.
12. Prior to Permit Utility plan showing location of existing and proposed water main and fire hydrant locations must be approved by the Fire Marshal and Utilities Engineer (City Code Sec. 6.20, Minnesota State Fire Code Sec. 508).



13. Prior to Permit Bicycle parking spaces must be provided and located throughout the site as approved by the City Engineer.
14. Prior to Permit Tier 2 Transportation Demand Management plan must be submitted (Sec. 21.301.09(b)(2)).
15. Prior to Permit Landscape plan must be approved by the Planning Manager and landscape surety must be filed (Sec 19.52).
16. Prior to Permit Parking lot and site security lighting plans must be provided and approved to satisfy the requirements of Section 21.301.07 of the City Code.
17. Prior to Permit A tax parcel combination of the subject properties must be completed to obtain legal zoning lot status.
18. Prior to C/O A Site Development Agreement incorporating all conditions of approval must be executed by the applicant and the City and must be properly recorded by the applicant with proof of recording provided to the Director of Community Development.
19. Prior to C/O The developer must submit electronic utility as-builts to the Public Works Department prior to the issuance of the Certificate of Occupancy.
20. Prior to C/O Building must be provided with an automatic fire sprinkler system as approved by the Fire Marshal (MN Bldg. Code Sec. 903, MN.Rules Chapter 1306; MN State Fire Code Sec. 903).
21. Prior to C/O Prior to occupancy, life safety requirements must be reviewed and approved by the Fire Marshal.
22. Ongoing The variance and building/site improvements are limited to those as shown on the approved plans in Case File #PL2016-147.
23. Ongoing Due to limited on-site parking, the Parish Hall (multi-purpose room) must not be used when the Sanctuary is in use.
24. Ongoing Five foot high, year-round perimeter screening must be provided along the western and southern property boundaries of the new parking lot as approved by the Planning Manager (Sec 19.52).
25. Ongoing Development must comply with the Minnesota State Accessibility Code.
26. Ongoing Alterations to utilities must be at the developer's expense.
27. Ongoing If the kitchen is used to serve any licensed activity, such as school, preschool or daycare, it must comply with current food code requirements.
28. Ongoing All construction stockpiling, staging, loading, unloading and parking must take place on-site and off adjacent public streets and public rights-of-way.
29. Ongoing All trash and recyclable materials must be stored inside the principal building (Sec. 19.51).

# City of Bloomington Notification Map



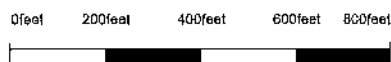
Notification Boundary  
Applicant Property  
Notified Properties

Zoning District Boundary  
(Labels Refer to Zoning District)

PL2016-147

(A notice of this application was sent to the registered owner of these properties)

Scale: 1:4800



Plot time: 08/26/2016 11:43:44

# AFFIDAVIT OF PUBLICATION

STATE OF MINNESOTA ) ss  
COUNTY OF HENNEPIN

Charlene Vold being duly sworn on an oath, states or affirms that he/she is the Publisher's Designated Agent of the newspaper(s) known as:

SC Bloomington

with the known office of issue being located in the county of:

HENNEPIN

with additional circulation in the counties of:

HENNEPIN

and has full knowledge of the facts stated below:

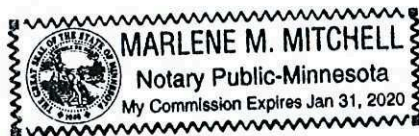
- (A) The newspaper has complied with all of the requirements constituting qualification as a qualified newspaper as provided by Minn. Stat. §331A.02.
- (B) This Public Notice was printed and published in said newspaper(s) once each week, for 1 successive week(s); the first insertion being on 09/15/2016 and the last insertion being on 09/15/2016.

**MORTGAGE FORECLOSURE NOTICES**  
Pursuant to Minnesota Stat. §580.033 relating to the publication of mortgage foreclosure notices: The newspaper complies with the conditions described in §580.033, subd. 1, clause (1) or (2). If the newspaper's known office of issue is located in a county adjoining the county where the mortgaged premises or some part of the mortgaged premises described in the notice are located, a substantial portion of the newspaper's circulation is in the latter county.

By: Charlene Vold  
Designated Agent

Subscribed and sworn to or affirmed before me on 09/15/2016 by Charlene Vold.

Marlene M Mitchell  
Notary Public



## Rate Information:

(1) Lowest classified rate paid by commercial users for comparable space:  
\$34.45 per column inch

Ad ID 594481

## CITY OF BLOOMINGTON NOTICE OF PUBLIC HEARING BY THE PLANNING COMMISSION

CASE FILE NUMBER:  
PL2016-147

APPLICANT: PEACE EVANGELICAL LUTHERAN CHURCH  
PROPERTY ADDRESSES:  
8600 East Bush Lake Road;  
7101 and 7103 West 86th Street  
PROPOSALS: Variance to reduce the required side yard setback from 60 feet to 30 feet for a building addition; Conditional Use Permit amendment for an addition to a place of assembly; and Final Site and Building Plans for a 2,774 square foot building addition and parking lot improvements.

DATE, LOCATION, AND TIME  
OF HEARING:  
09/29/2016, 6:00 p.m.  
City Council Chambers -  
Bloomington City Hall  
1800 West Old Shakopee Road  
City Council consideration date is announced at the Planning Commission meeting date at which action is taken.

HOW YOU CAN PARTICIPATE:  
(Please include Case File number above when corresponding)  
1. Submit a letter to the address below expressing your views;  
2. Attend the hearing and give testimony about the proposal; and/or  
3. Contact the Planning Division using the information below.

FURTHER INFORMATION:  
Nick Johnson, Planner  
1800 West Old Shakopee Road  
Bloomington, MN 55431-3027  
Phone: 952-563-8925  
Email:  
nmjohnson@BloomingtonMN.gov

Published in the  
Bloomington Sun Current  
September 15, 2016  
594481

## **Introduction**

Peace Lutheran Church is located at 8600 East Bush Lake Road, Bloomington, MN 55438. Peace has been an integral part of our Bloomington neighborhood for over 35 years! Over those 35 years, our congregation has seen its share of change. We currently find our community in need of a remodel to our existing space, an expansion of parking and a modest addition of square footage. These proposed changes are described below in the proposed work section. Additionally, we are requesting an amendment to our existing CUP to add non-concurrent use agreement and a variance to building setback. The details supporting these requests are outlined below.

## **Building and Site Background**

The first phase of construction was in 1980 and included a sanctuary (currently the Parish Hall), narthex, offices and classrooms.

An addition was constructed in 1989 that included a new Sanctuary, enlarged narthex, kitchen and lower level classrooms. The existing Sanctuary was re-purposed to serve as the Parish Hall.

A remodeling of the narthex was completed in 2002. This remodel did not include any additional square footage.

The adjacent property to the northwest that includes a duplex (7101/7301 W. 86<sup>th</sup> Street) was purchased by the congregation in 1997 and has served as a program space. The duplex was constructed in 1978.

## **Description of Proposed Work**

### ***Interior***

The congregation requests approval to remodel existing construction to provide four classrooms on the upper level and four classrooms and a nursery on the lower level. One of the lower level classrooms is open to the corridor. There is currently one classroom on the upper level and four classrooms and a nursery on the lower level. The existing kitchen will be remodeled to serve as a sacristy and storage.

New construction will include administration space, kitchen, toilets and storage.

Existing upper level area is 9,008 SF and existing lower level area is 2,949 SF. Total existing area is 11,957 SF.

Area of proposed new construction is 2,774 SF. Total area of proposed building is 14,731 SF.

### ***Current Parking***

The existing parking lot currently has 92 stalls. Our current parking lot is adequate for the vast majority of services and events held at Peace. The average yearly breakdown for services is:

- 110 (96%) of the services are regular Sunday or midweek services. These services rarely result in any overflow of the current lot.
- 5 (4%) of these services are Easter Sunday, Christmas Eve and an occasional very well-attended funeral service. These services may result in overflow of the current parking lot which results in parking on 86<sup>th</sup> and the surrounding neighborhood streets.

While our lot is adequate the majority of time, we want to remain good neighbors and respond to the times when parking is not adequate. Therefore, we are proposing additional parking.

### ***Proposed Parking***

The existing parking lot will be modified and the duplex will be demolished and the land will be used to provide additional parking/proof-of-parking.

The modification of the current lot will result in 73 stalls and 7 proof of parking stalls on the northwest corner of the lot. The addition of the upper lot will result in 45 parking stalls with 9 proof of parking stalls on the east side. The total will be 134 stalls; 118 now with an additional 16 proof of parking stalls that could be added later if necessary.

The proposed addition of 2,774 square feet requires Peace to add 5 parking stalls. However, we are seeking to add a total of 26 spaces; 21 more than required.

### ***Non-Concurrent Use Agreement***

Current parking requirements indicate that Peace would need to have 185 stalls for the building, including the proposed 2,774sf addition. The Peace Lutheran site is not able to support 185 parking stalls and we are requesting a non-concurrent use agreement. The reasons that we are making this request are as follows:

1. The manner in which Peace utilizes the building is reasonable and is supported by fewer number of parking stalls. The normal activities, worship, education and events, are conducted non-concurrently. We do not and will not have activities that use all areas of the building at the same time. We have put together the three most common building use scenarios to illustrate what areas of the building are used and how many parking stalls are necessary. In each of these scenarios, our proposed parking plan will provide more than enough stalls. **Please see the parking space calculations and the use scenarios.**
  - Scenario 1: Regular Sunday Services  
Currently, we do not hold any education during the worship services and therefore, the Sanctuary and nursery are the areas utilized during the worship services. The number of parking stalls required for this is 100. Education is held between or after services in the Parish Hall, classrooms and nursery. The number of stalls required for this is 20. If we were to move to concurrent worship and education, the maximum number of stalls needed would be 120.
  - Scenario 2: Christmas Eve and Easter Sunday  
We do not have any education taking place on these Holidays and therefore, we use only the Sanctuary and the nursery. For these well attended services, we may have overflow seating in the south section of the narthex. When the Sanctuary, nursery and south narthex are fully used, 118 parking stalls are required.
  - Scenario 3: Visitation or Wedding non-ceremony space  
We use the upper level areas for funeral (and very rarely wedding) receptions. These areas include the Parish Hall, south and mid narthex and the upper level classrooms. These receptions take place after the ceremony so the Sanctuary is not used concurrently with the reception space. When these areas are fully utilized, 84 parking stalls are required.
2. Granting the non-concurrent use agreement, allows us to double the number of current stalls and assure adequate available parking for our needs. If the non-concurrent use agreement isn't granted and we remain at the current number of stalls, we will not have addressed the needs on Christmas Eve and Easter Sunday.

### ***Setback Variance***

In 1980, when the original structure was built, Peace was granted building setback variance from East Bush Lake Road. The variance reduced the setback from 60' to 34'. The addition of the 2,774 square feet will be on the north and the east side on the current building. For the following reasons, we are requesting that the new building addition also be granted a variance from the 60' setback.

1. Our drawings show that the setback for the addition is 35' from East Bush Lake Road; within the 34' variance that was granted in 1980.
2. This variance would allow consistency of the building plan and footprint. The addition would become a harmonious piece of the existing building. The building will not appear to be any closer to East Bush Lake Road than the current variance already allows.
3. The unique angle shape of the property makes it difficult to retain the 60' setback. The 'stepped' form of the building is such that the setback varies, with the closest distance from the building to the property line being 34'.
4. The additional right of way on the county road makes it difficult to retain the 60" setback.

### **Attachments**

- A0.1 - Code Review
- A0.2 - Code Plan
- A1.0 - Site Plan
- A1.1 - Axonometric Views
- A2.0 - Main Level Floor Plan
- A2.1 - Lower Level Floor Plan
- A5.0 - Exterior Elevations
- A5.1 - Exterior Elevations
- C1.0 to C6.0 – Civil Engineering Drawings
- L100 to L103 – Landscaping Drawings
- Proposed Easement Vacation Sketch
- Parking space calculations and concurrent use scenarios

| Sanctuary Occupant Load Determination Options   |  | Area  | Occupant Load Factor | Occupants | Required Parking Spaces | Use Scenarios  |   |  |
|---|--|-------|----------------------|-----------|-------------------------|--|---|--|
|   |  |       |                      |           |                         | Scenario 1- Worship Use- Reguar Sundays and midweek services | Scenario 2- Worship Use- Christmas and Easter | Scenario 3- Visitation or Wedding-Non Ceremony Space |
|   |  |       |                      |           |                         |  |   |  |
|   | Lower Level  |       |                      |           |                         |  |   |  |
|   | Nursery  | 327   | 800                  | 0.41      | 0.14                    | 0.14   | 0.14  | 0.14   |
|   | Lounge   | 230   | 400                  | 0.58      | 0.19                    | 0.19   |   | 0.19   |
|   | Classroom A  | 332   | 800                  | 0.42      | 0.14                    | 0.14   |   |  |
|   | Classroom B  | 300   | 800                  | 0.38      | 0.13                    | 0.13   |   |  |
|   | Classroom C  | 238   | 800                  | 0.30      | 0.10                    | 0.10   |   |  |
|   |  |       |                      |           |                         |  |   |  |
|   | Main Level   |       |                      |           |                         |  |   |  |
|   | Classroom A*   | 300   | 15                   | 20.00     | 6.67                    | 6.67   |   | 6.67   |
|   | Classroom B*   | 291   | 15                   | 19        | 6.33                    | 6.33   |   | 6.33   |
|   | Classroom C  | 379   | 800                  | 0.47      | 0.16                    | 0.16   |   |  |
|   | Classroom D  | 406   | 800                  | 0.51      | 0.17                    | 0.17   |   |  |
|   | Coats  | 165   | 0                    |           |                         |  |   |  |
|   | Narthex  | 2,547 |                      |           |                         |  |   |  |
|   | Narthex (South)  | 824   | 15                   | 54.93     | 18.31                   |  | 18.31   | 18.31  |
|   | Narthex (Mid)  | 520   | 15                   | 34.67     | 11.56                   |  |   | 11.56  |
|   | Narthex (North Circulation)  | 1,203 | 0                    |           |                         |  |   |  |
|   | Parish Hall  | 1,611 | 15                   | 107.40    | 35.80                   |  |   | 35.80  |
| Option 1-Not Used   | Sanctuary Seating (By area-2,717 / 15 = 181.13 / 3 = 60.38 stalls) | 2,717 | 15                   | 181.13    | 60.38                   |  |   |  |
| Option 2 -Used  | Sanctuary Open (Chancel-144/15 = 9.60 / 3 = 3.20)                  | 144   | 15                   | 9.60      | 3.20                    | 3.20   | 3.20  |  |
|   | Sanctuary (determined by number of seats = 262)                    |       |                      | 262       | 87.33                   | 87.33  | 87.33   |  |
|   | Sanctuary (replace music with seating 48-20=additional 28)         |       |                      | 28        | 9.33                    | 9.33   | 9.33  |  |
|   | Sacristy   | 228   | 100                  | 2.28      | 0.76                    | 0.76   |   |  |
|   | Mother's Room  | 102   | 100                  | 1.02      | 0.34                    | 0.34   |   |  |
|   | Offices  | 920   | 1 Space/285          |           | 3.2                     | 3.2  |   | 3.2  |
|   | Kitchen  | 425   | 100                  | 4.25      | 1.42                    | 1.42   |   | 1.42   |
|   | Storage  | 280   | 0                    |           |                         |  |   |  |
|   |  |       |                      |           |                         |  |   |  |
|   | Total  |       |                      |           | 185.30                  | 119.63   | 118.31  | 83.64  |
|   |  |       |                      |           |                         |  |   |  |
| * These spaces are used by adults and children so we opted to use the adult calculation for parking |  |       |                      |           |                         |  |   |  |

## Use Scenarios

### Scenario 1

#### Regular Sunday Services

Currently we use the Sanctuary and the nursery which requires 100 parking spaces.

If in the future we begin to use other spaces (education, kitchen etc.) we would need up to 120 parking spaces.

### Scenario 2

#### Easter and Christmas Services (and very rarely a funeral service)

We use the Sanctuary, the south Narthex for overflow and the nursery which requires 118 parking spaces.

### Scenario 3

#### Gathering/reception space for funerals and weddings

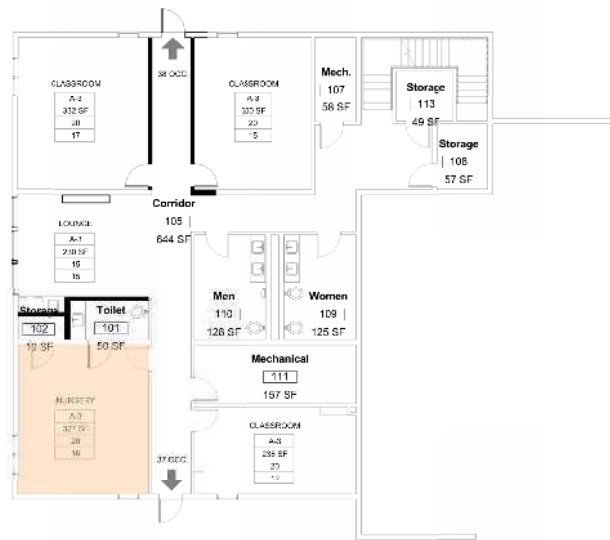
For funerals (and very rarely a wedding), we use the several gathering areas for the reception. These gatherings require 84 parking spaces.

We do not use the Sanctuary and these gathering spaces at the same time. People move from the service to the reception.





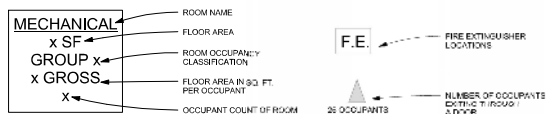




0' 10' 20' 30' 40' 50'

## 2 Lower Level Code Plan

### Scenario 2 Worship Use-Christmas & Easter 07 29 16



## Code Plan Key

1" = 20'-0"

## APPLICABLE CODES

2015 MINNESOTA BUILDING CODE INCORPORATING:  
2012 INTERNATIONAL BUILDING CODE

2012 INTERNATIONAL ENERGY CONSERVATION CODE  
2012 INTERNATIONAL MECHANICAL CODE  
2012 INTERNATIONAL FUEL GAS CODE  
2012 INTERNATIONAL PLUMBING CODE  
2011 NATIONAL ELECTRIC CODE

GROSS FLOOR AREA (IBC Sec. 1002.1)

LOWER LEVEL GROSS AREA 2,979 SF  
MAIN LEVEL GROSS AREA (EXIST) 9,008 SF  
MAIN LEVEL GROSS AREA (NEW) 2,847 SF

TOTAL LOWER LEVEL GROSS AREA: 2,979 SF  
TOTAL MAIN LEVEL GROSS AREA: 11,855 SF

TOTAL BUILDING GROSS AREA: 14,834 SF

NET FLOOR AREA (IBC Sec. 1002.1)

LOWER LEVEL NET AREAS:

NURSERY 327 SF  
LOUNGE 230 SF  
CLASSROOM A 332 SF  
CLASSROOM B 300 SF  
CLASSROOM C 238 SF

TOTAL LOWER LEVEL NET AREA: 1,427 SF

MAIN LEVEL NET AREAS:

CLASSROOM A 300 SF  
CLASSROOM B 291 SF  
CLASSROOM C 378 SF  
CLASSROOM D 408 SF  
COATS 165 SF  
NARTHEX 2,547 SF  
PARISH HALL 2,717 SF  
SANCTUARY 228 SF  
MOTHER'S ROOM 102 SF  
OFFICES 120 SF  
KITCHEN 425 SF  
GARAGE 285 SF

TOTAL MAIN LEVEL NET AREA: 10,376 SF

OCCUPANCY TYPE (IBC Sec. 303)

LOWER LEVEL ROOMS TYPE A-3

MAIN LEVEL ROOMS TYPE A-3

OCCUPANT LOAD PER IBC (IBC Table 1004.1.2)

LOWER LEVEL OCC. LOAD:

NURSERY 327 SF/20 = 16  
LOUNGE 230 SF/15 = 15  
CLASSROOM A 332 SF/20 = 17  
CLASSROOM B 300 SF/20 = 15  
CLASSROOM C 238 SF/20 = 12

TOTAL LOWER LEVEL OCC. LOAD: 75 OCCUPANTS

MAIN LEVEL OCC. LOAD:

CLASSROOM A 300 SF/20 = 15  
CLASSROOM B 291 SF/20 = 15  
CLASSROOM C 378 SF/20 = 19  
CLASSROOM D 408 SF/20 = 20  
COATS 165 SF/15 = 11  
NARTHEX 2,222 SF/15 = 148  
PARISH HALL 1,611 SF/15 = 107  
SANCTUARY 2,717 SF/15 = 181  
SACRISTY 228 SF/100 = 2  
MOTHER'S ROOM 102 SF/100 = 1  
OFFICES 120 SF/100 = 1  
KITCHEN 425 SF/200 = 2

TOTAL MAIN LEVEL OCC. LOAD: 530 OCCUPANTS

TOTAL BUILDING OCCUPANT LOAD: 605 OCCUPANTS

ACCESSORY OCCUPANCIES (IBC Sec. 508.2)

TOTAL MAIN LEVEL AREA: 10,376 SF  
TOTAL MAIN LEVEL B AREA: 920 SF  
B AREA LESS THAN 10% OF TOTAL MAIN LEVEL BLDG. AREA  
B OCCUPANCY IS ACCESSORY TO A-3 OCCUPANCY

OCCUP. SEPARATION (IBC Sec. 508.3)

NO SEPARATION REQUIRED - NON-SEPARATED USE

CORRIDOR FIRE-RESISTANCE (IBC Sec. 1018)

REQUIRED FIRE-RESISTANCE RATING (HOURS)

FOR OCCUPANCY TYPE A-3 AND B: 0 HOURS

"BUILDING IS EQUIPPED THROUGHOUT WITH  
AUTOMATIC SPRINKLER SYSTEM"

INCIDENTAL USE SEPARATION (IBC Table 509)

NONE REQUIRED - BUILDING IS EQUIPPED THROUGHOUT  
WITH AUTOMATIC SPRINKLER SYSTEM

MIN. EGRESS REQ. (IBC Table 1015.1)

SEE EXIT DIAGRAM, SHEET A0.1 AND A0.2, FOR SPECIFIC  
INFORMATION

BUILDING  
3 EXITS REQUIRED, 3 EXITS ARE PROVIDED

MIN. EXIT WIDTH (IBC TABLE 1005.1, P.205)

SEE EXIT DIAGRAM, SHEET A0.1 AND A0.2, FOR SPECIFIC  
INFORMATION

LOWER LEVEL: 75 OCCS. X 0.2" = 15" REQ'D, 68" PROVIDED  
AT STAIRWAY: 75 OCCS. X 0.3" = 23" REQ'D, 48" PROVIDED

MAIN LEVEL: 535 OCCS. X 0.2" = 107" REQ'D

CONSTRUCTION TYPES (IBC Sec. 602)

R III DIN: TYPE V-R

FIRE RESISTANCE-BLDG. (IBC Table 601)

BUILDING = 0 HRS.

FIRE RESISTANCE-CORRIDORS (IBC Table 1018.1)

OCCUPANCY A AND B W/ SPRINKLER SYSTEM = 0 HRS.

FIRE RESISTANCE-EXT. WALLS (Table 602)

FIRE SEPARATION DISTANCE IS GREATER THAN  
10', SO 0 HRS. RATING IS REQUIRED

ALL CRAWL SPACE AREAS (IBC Table 503)

USE MOST RESTRICTIVE BETWEEN TYPES A-3 & B

TYPE A-3 IS MOST RESTRICTIVE

TABULAR ALLOWED=8000 SQ.FT./FLR. @ 1 STORY ABOVE GRADE

MAIN LEVEL, 1 STORY ABOVE GRADE

GROSS FLOOR AREA = 11,855 SF

HEIGHT INCREASE (IBC Sec. 504)

BUILDING HEIGHT IS LESS THAN 40' 50' HEIGHT INCREASE  
CALCULATIONS ARE NOT NECESSARY

AREA INCREASE (IBC Sec. 508.3)

INCREASE FOR SPRINKLER SYSTEM:  $I_s = 3$

INCREASE FOR FRONTAGE: NOT REQ'D

PERMITTED INCREASE 6000 X 3 = 18,000 SF

MAIN LEVEL GROSS FLOOR AREA 11,855 SF

BUILDING IS WITHIN PERMITTED INCREASE

MIN. PLUMBING FIXTR. (IBC Table 2902.1, p.521)

TOTAL BUILDING OCCUPANT LOAD = 605 605/2 = 303 PER SEX

303/75 = 4 TOILETS REQUIRED FOR WOMEN

303/150 = 2 TOILETS REQUIRED FOR MEN

303/200 = 2 LAVATORIES PER ROOM REQUIRED

SERVICE SINK: 1 REQUIRED

TOTAL PLUMBING FIXTURES REQUIRED:

4 TOILETS ARE REQUIRED FOR WOMEN

5 TOILETS ARE PROVIDED

2 TOILETS ARE REQUIRED FOR MEN

5 TOILETS ARE PROVIDED

2 LAVATORIES ARE PROVIDED IN EACH RESTROOM

DRINKING FOUNTAINS

1 DRINKING FOUNTAIN IS PROVIDED

FIRE ALARM AND DETECTION (IBC Sec. 907)

TYPE A-3 NO MANUAL ALARM SYSTEM REQUIRED

TYPE B NO MANUAL ALARM SYSTEM REQUIRED

SPRINKLER (IBC Sec. 903.2.1.3)

THE BUILDING IS FULLY-EQUIPPED WITH AN  
AUTOMATIC SPRINKLER SYSTEM

## 1 Code Review

12" = 1'-0"

## Peace Lutheran Church

8600 E. Bush Lake  
Rd. Bloomington, MN  
55438

Project Status

PROGRESS SET  
NOT FOR CONSTRUCTION  
June 25 2016

XXXXXXXXXXXXXXXXXXXX  
Registration No.  
SEAL

Issue Date

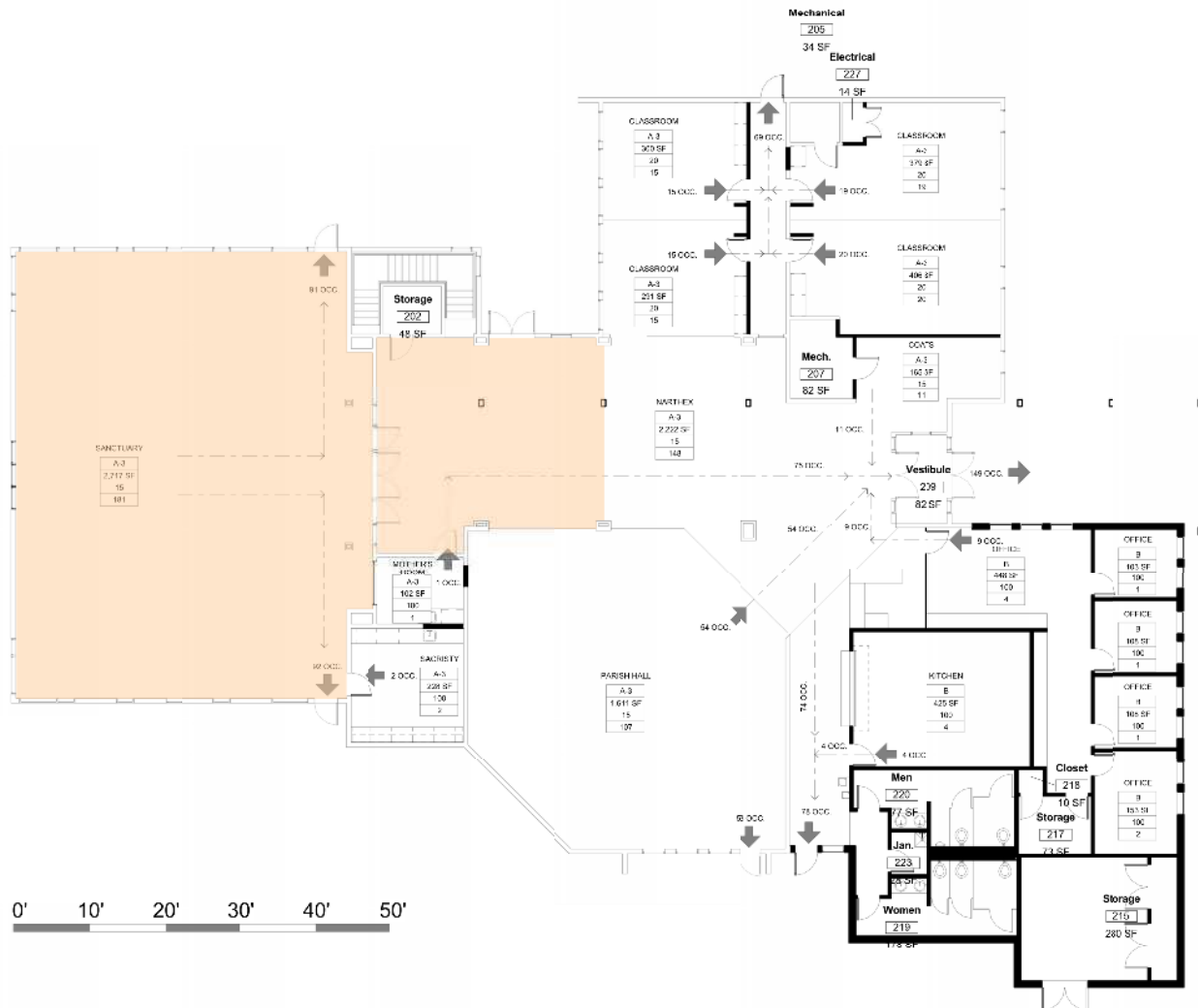
PROJ. NO. 1511.00

## Code Review

TITLE

SHEET NO.

A0.1



0' 10' 20' 30' 40' 50'

1 Main Level Code Plan  
1/8" = 1'-0"

**Scenario 2**  
**Worship Use-Christmas & Easter**  
**07 29 16**



Code Plan Key  
1" = 20'-0"

Peace Lutheran Church

8600 E. Bush Lake Rd. Bloomington, MN 55438

Project Status

PROGRESS SET  
NOT FOR CONSTRUCTION  
June 25 2016

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Registered Architect under the laws of the State of Minnesota.

XXXXXX Registration No.  
SEAL

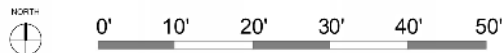
DATE Issue Date

PROJ. NO. 1511.00

Code Plan

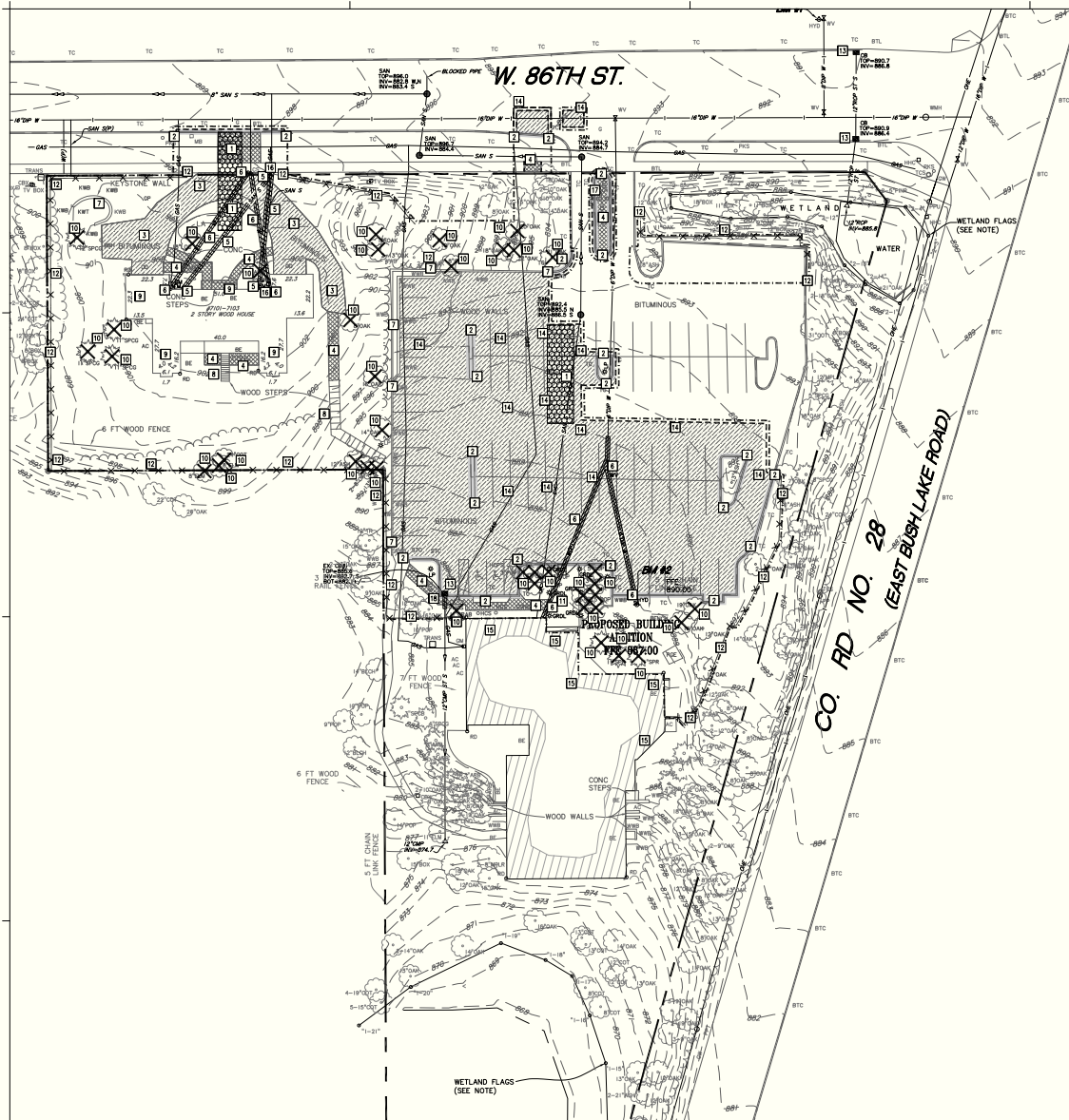
TITLE

SHEET NO. A0.2









#### PROPOSED PLAN SYMBOLS

|                            |     |
|----------------------------|-----|
| CONSTRUCTION LIMITS        | --- |
| SILTATION FENCE            | --- |
| PROPERTY LINE              | --- |
| SAWCUT LINE (APPROX.)      | --- |
| ROCK CONSTRUCTION ENTRANCE | --- |
| BITUMINOUS REMOVAL         | --- |
| CONCRETE REMOVAL           | --- |
| PIPE REMOVAL               | --- |
| CURB REMOVAL               | --- |
| TREE REMOVAL               | --- |

NOTE:  
STORM SEWER INLETS NOT SHOWN ON PLAN MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INSTALL INLET SEDIMENT PROTECTION PER DETAIL 1/C5.0 ON ALL STORM INLETS THAT MAY RECEIVE RUNOFF.

#### KEYED NOTES

KEYED NOTES ARE DENOTED BY [ ] ON PLAN.

1. INSTALL ROCK CONSTRUCTION ENTRANCE. REFER TO DETAIL 4/C5.1.
2. REMOVE CURB AND GUTTER IN ITS ENTIRETY TO THE EXTENT SHOWN. SAWCUT AND REMOVE AT NEAREST JOINT.
3. REMOVE BITUMINOUS PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
4. REMOVE CONCRETE PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
5. REMOVE SANITARY SEWER IN ITS ENTIRETY TO THE EXTENTS SHOWN. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
6. REMOVE WATER MAIN, HYDRANTS, AND VALVES IN ITS ENTIRETY TO THE EXTENTS SHOWN. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
7. REMOVE RETAINING WALL IN ITS ENTIRETY.
8. REMOVE WOOD STEPS IN ITS ENTIRETY.
9. REMOVE BUILDING IN ITS ENTIRETY, INCLUDING EXTERIOR STAIRS, OVERHANGS, FOOTINGS, AND SMALL UTILITIES. COORDINATE REMOVAL OF SMALL UTILITIES WITH LOCAL UTILITY COMPANIES.
10. REMOVE TREE IN ITS ENTIRETY INCLUDING STUMP.
11. REMOVE GROUND LIGHTS AND LIGHT POLE.
12. INSTALL SILT FENCE. REFER TO DETAIL 3/C5.1.
13. INSTALL INLET SEDIMENT PROTECTION, ONLY BASKET OR SACK STYLE IN TRAFFIC AREAS. REFER TO DETAIL 1/C5.0.
14. SAWCUT AND REMOVE BITUMINOUS PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
15. EXISTING BUILDING TO REMAIN. PROTECT AT ALL TIMES.
16. REMOVE NATURAL GAS LINE IN ITS ENTIRETY TO THE EXTENTS SHOWN. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
17. REMOVE EXISTING SIGN AND LIGHT POLES.
18. EXISTING CATCH BASIN TO BE RECONSTRUCTED. SEE UTILITY PLAN SHEET C3.0 FOR REFERENCE.

#### ABBREVIATIONS

|      |                          |
|------|--------------------------|
| BDS  | Building                 |
| BM   | Bench Mark               |
| CB   | Catch Basin              |
| CONC | Concrete                 |
| DIP  | Ductile Iron Pipe        |
| ELEV | Elevation                |
| EX   | Existing                 |
| FTE  | Finished Floor Elevation |
| IN   | Inlet                    |
| MAX  | Maximum                  |
| MIN  | Minimum                  |
| PVC  | Polyvinyl Chloride       |
| RCP  | Reinforced Concrete Pipe |

#### DEMOLITION AND REMOVAL NOTES:

1. PRIOR TO START OF DEMOLITION, ALL EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE INSTALLED BY THE CONTRACTOR AND INSPECTED BY THE CITY OF BLOOMINGTON. ALL SILT FENCES SHALL BE INSTALLED AND INSPECTED PRIOR TO ANY CONSTRUCTION ACTIVITY. SILT FENCES SHALL BE INSTALLED ALONG THE CONTOUR.
2. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE LOCATION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY THE LOCATION, ELEVATION AND MARK ALL EXISTING UTILITIES 48 HOURS BEFORE CONSTRUCTION STARTS. THE ENGINEER, ARCHITECT OR OWNER DOES NOT GUARANTEE THAT ALL THE UTILITIES ARE MAINTAINED, OR IF MAINTAINED, ARE SHOWN CORRECTLY. CONTRACTOR SHALL ONE CALL AT 651-434-5002 FOR FIELD LOCATING EXISTING UTILITIES. CONTACT UTILITY OWNER IF DAMAGE OCCURS DUE TO CONSTRUCTION.
3. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR IS TO CLEARLY IDENTIFY IN THE FIELD THE WETLAND PERIMETERS THAT ARE NOT TO BE IMPACTED SO THAT NO ENCROACHMENT OCCURS. AFTER THE PERIMETERS ARE CLEARLY MARKED IN THE FIELD, THE CONTRACTOR SHALL CONTACT NINE-MILE CREEK WATERSHED DISTRICT TO CONFIRM AND APPROVE. NO ENCROACHMENT LIMITS.
4. THERE MAY BE MISCELLANEOUS ITEMS TO BE REMOVED THAT ARE NOT IDENTIFIED ON THESE PLANS. THE CONTRACTOR SHALL VISIT THE SITE AND REVIEW THE DOCUMENTS TO OBTAIN A CLEAR UNDERSTANDING OF THE INTENDED SCOPE OF WORK.
5. PRIOR TO START OF CONSTRUCTION, DISCONNECT ALL GAS AND ELECTRIC SERVICES. COORDINATE DISCONNECTION OF EACH UTILITY WITH THE UTILITY OWNER. REMOVE ALL GAS AND ELECTRIC LINES UNDER PROPOSED BUILDING FOOTPRINT.
6. ANY UTILITIES NOT INDICATED FOR REMOVAL OR ABANDONMENT, ARE TO BE PROTECTED AT ALL TIMES.
7. EXISTING CURB AND GUTTER IS TO BE REMOVED WITHIN THE SCOPE OF THE PROJECT FROM THE SAW CUT LINES TO THE NEAREST JOINT.
8. THE BACKGROUND INFORMATION WAS PREPARED BY SUNDE LAND SURVEYING (952) 881-2455.
9. ALL WORK IN THE PUBLIC RIGHT OF WAY IS TO BE COORDINATED WITH THE CITY OF BLOOMINGTON. ROADWAY REPAIRS, BOLLARD REPAIRS, AND TRAFFIC CONTROL ARE TO BE PER CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.

#### GENERAL NOTES:

1. CONCRETE CURB AND GUTTER REMOVAL, PAVEMENT REMOVAL, AND UTILITY REMOVAL LIMITS ARE TO BE COORDINATED WITH THE CITY OF BLOOMINGTON AND UTILITY OWNER. REFER TO ALL CONSTRUCTION DOCUMENTS.
2. THE CONTRACTOR SHALL DEVELOP AND IMPLEMENT A TRAFFIC CONTROL PLAN WHILE WORKING WITHIN THE RIGHT-OF-WAY. THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE CITY OF BLOOMINGTON ENGINEERING DEPARTMENT PRIOR TO STREET ENCROACHMENT.
3. CONTRACTOR SHALL VISIT THE SITE PRIOR TO BIDDING AND REVIEW ALL CONSTRUCTION DOCUMENTS AND GEOTECHNICAL REPORTS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ITEMS THAT SHOULD HAVE BEEN ANTICIPATED BY PERFORMING THE ABOVE.
4. THE ROCK CONSTRUCTION ENTRANCE INDICATED ON THE PLAN IS SHOWN IN AN APPROXIMATE LOCATION. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR IS TO COORDINATE WITH THE CITY OF BLOOMINGTON FOR THE EXACT ROCK CONSTRUCTION ENTRANCE LOCATION.

#### CITY OF BLOOMINGTON NOTES:

1. UTILITY AS-BUILTS MUST BE PROVIDED PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY.
2. CONTRACTOR SHALL OBTAIN A PUBLIC WORKS PERMIT FOR OBSTRUCTIONS AND CONCRETE WORK WITHIN THE RIGHT-OF-WAY. PERMIT IS REQUIRED PRIOR TO REMOVALS OR INSTALLATION. CONTACT BRIAN HANSEN (952-563-4543, BHANSEN@BLOOMINGTONMN.GOV) FOR PERMIT INFORMATION.
3. CONTRACTOR SHALL OBTAIN A PUBLIC WORKS PERMIT FOR UNDERGROUND WORK WITHIN THE RIGHT-OF-WAY. PERMIT IS REQUIRED PRIOR TO REMOVALS OR INSTALLATION. CONTACT UTILITIES (952-563-4568) FOR PERMIT INFORMATION.
4. TEMPORARY STREET SIGNS, LIGHTING, AND ADDRESSES SHALL BE PROVIDED DURING CONSTRUCTION.
5. ALL CONSTRUCTION AND POST-CONSTRUCTION PARKING AND STORAGE OF EQUIPMENT AND MATERIALS MUST BE ON-SITE. USE OF PUBLIC STREETS OR PUBLIC RIGHT-OF-WAY FOR PRIVATE CONSTRUCTION PARKING, LOADING/UNLOADING, AND STORAGE WILL NOT BE ALLOWED.
6. ALL PUBLIC SIDEWALKS SHALL NOT BE OBSTRUCTED.
7. USE CLASS 52 DIP WATER MAIN FOR PIPE 12-INCHES IN DIAMETER AND SMALLER. A MINIMUM 8 MIL POLYWRAP IS REQUIRED ON ALL DIP.
8. TAPS OF LIVE WATER MAINS ARE DONE BY CITY FORCES AND PAID FOR AND COORDINATED WITH THE CONTRACTOR.
9. UTILITY AND MECHANICAL CONTRACTORS SHALL COORDINATE INSTALLATION OF WATER SERVICE PIPES, FITTINGS, AND VALVES ALL THE WAY INTO THE BUILDING (I.E. UP TO METERS AND/OR FIRE SERVICE EQUIPMENT) TO ACCOMMODATE CITY INSPECTION AND TESTING. UTILITY AND MECHANICAL CONTRACTORS SHALL COORDINATE INSTALLATION OF SEWER SERVICES ALL THE WAY INTO THE BUILDING TO ACCOMMODATE CITY INSPECTION AND TESTING.
10. COMBINATION FIRE AND DOMESTIC SERVICES MUST TERMINATE WITH A THREAD ON FLANGE OR A 1/4" TO FLANGE ADAPTER.
11. ALL COMPONENTS OF THE WATER SYSTEM, UP TO THE WATER METER OR FIRE SERVICE EQUIPMENT MUST UTILIZE PROTECTIVE INTERNAL COATINGS MEETING CURRENT ANSI/AWWA STANDARDS FOR CEMENT MORTAR LINING OR SPECIAL COATINGS. THE USE OF UNLINED OR UNCOATED PIPE IS NOT ALLOWED.
12. UTILITY PERMITS ARE REQUIRED FOR CONNECTIONS TO THE PUBLIC STORM, SANITARY, AND WATER SYSTEM. CONTACT UTILITIES (952-563-8777) FOR PERMIT INFORMATION.
13. NO BALES ALLOWED FOR INLET PROTECTION AND/OR DITCH CHECKS.

1  
C1.0 DEMOLITION AND EROSION CONTROL PLAN  
1"=30'

Peace Lutheran Church

8600 E. Bush Lake Rd. Bloomington, MN 55438

**BKBM**  
BROOKLYN KENNEDY BROWNE & BROWN  
1920 Brooklyn Boulevard  
Bloomington, MN 55405-0818  
Phone (763) 544-0500  
Fax (763) 544-0400  
www.bkbkm.com  
© 2014 BKBKM Engineers, Inc.  
All rights reserved.  
This document is an instrument of service and is the property of BKBKM Engineers, Inc. and may not be used or reproduced without prior written consent. BKBKM PROJECT NO. 16359.00

Project Status

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota.

Joel Maier 19181  
Registration No.

SEAL

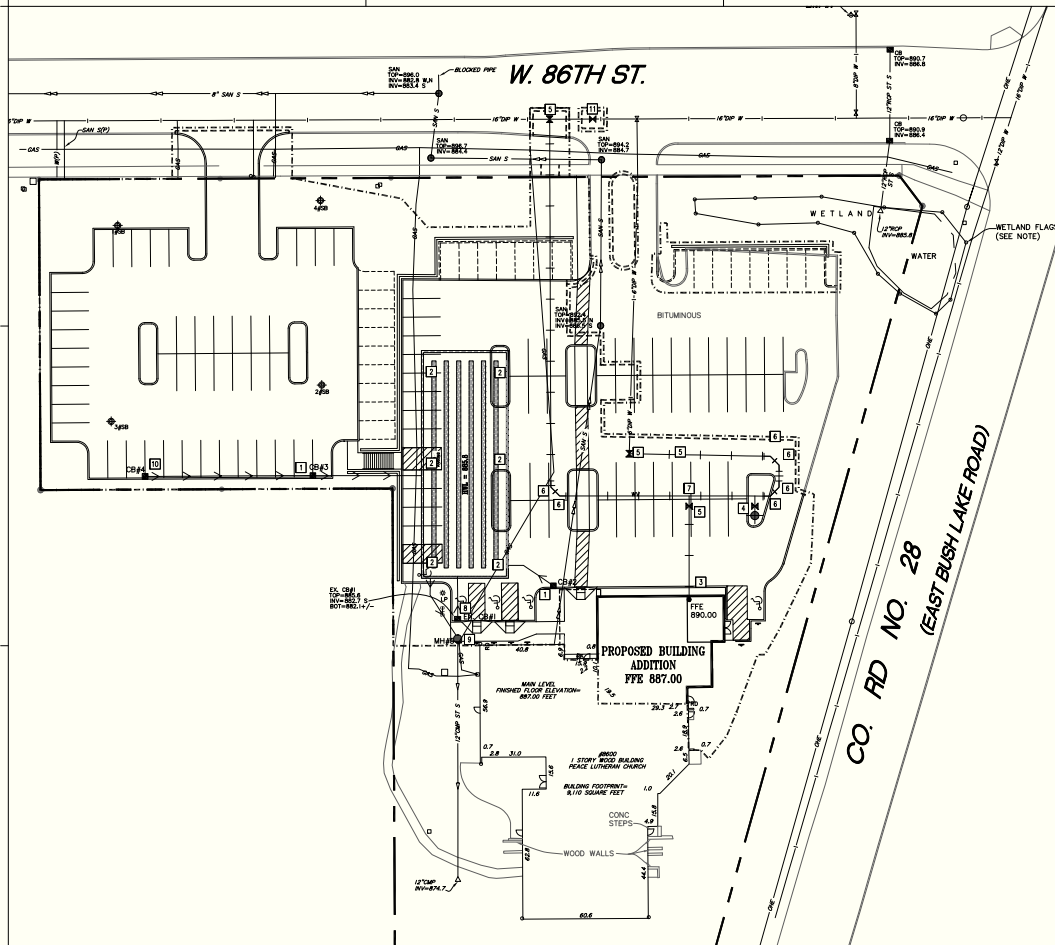
DATE 8/24/2016

PROJ. NO. 16359.00

DEMOLITION AND EROSION CONTROL PLAN

TITLE

SHEET NO. C1.0

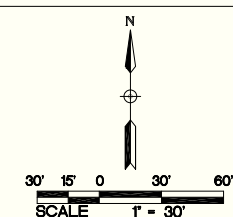


# PROPOSED PLAN SYMBOLS

|                       |     |
|-----------------------|-----|
| CONSTRUCTION LIMITS   | --- |
| PROPERTY LINE         | --- |
| SAWCUT LINE (APPROX.) | --- |
| WATER PIPE            | --- |
| STORM SEWER           | --- |
| RETAINING WALL        | --- |
| FIRE HYDRANT          | --- |
| GATE VALVE            | --- |
| CATCH BASIN           | --- |
| MANHOLE               | --- |

# ABBREVIATIONS

|        |                           |
|--------|---------------------------|
| BUILDG | Building                  |
| BM     | Bench Mark                |
| CB     | Catch Basin               |
| CMP    | Corrugated Metal Pipe     |
| CONC   | Concrete                  |
| DIP    | Ductile Iron Pipe         |
| ELEV   | Elevation                 |
| EX     | Existing                  |
| FTE    | Finished Floor Elevation  |
| HDP    | High Density Polyethylene |
| INV    | Invert                    |
| MAX    | Maximum                   |
| MH     | Manhole                   |
| MIN    | Minimum                   |
| MJ     | Mechanical Joint          |
| PV     | Post Indicator Valve      |
| PVC    | Polyvinyl Chloride        |
| RCP    | Reinforced Concrete Pipe  |
| RED    | Reducer                   |
| RD     | Roof Drain                |



# KEYED NOTES

- KEYED NOTES ARE DENOTED BY [KEY] ON PLAN.
- INSTALL SUMP CATCH BASIN. REFER TO DETAIL 3/C3.0.
- INSTALL INFILTRATION SYSTEM PER DETAIL 2/C3.0. INFILTRATION SYSTEM SHALL HAVE AN INFILTRATION VOLUME OF 3,678 CUBIC FEET A TOTAL VOLUME OF 12,816 CUBIC FEET, AND MEET THE RATE CONTROL REQUIREMENTS INDICATED ON SHEET C3.0. AN ALTERNATE DESIGN MUST BE APPROVED BY CIVIL ENGINEER PRIOR TO CONSTRUCTION.
- STUB 6-INCH DOMESTIC WATER SERVICE INTO PROPOSED BUILDING. PROVIDE THREAD ON FLANGE OR MJ TO FLANGE ADAPTER AT 12-INCHES ABOVE FFE. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
- INSTALL FIRE HYDRANT AND GATE VALVE. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
- INSTALL 6-INCH GATE VALVE. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
- INSTALL 6-INCH 45-DEGREE BEND WITH THRUST BLOCKING. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
- INSTALL 6-INCH BY 6-INCH TEE. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
- RECONSTRUCT EXISTING CATCH BASIN FOR PROPOSED PIPE CONNECTION. GROUT SEAL AROUND OPENING. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
- CONSTRUCT MHS OVER EXISTING 12-INCH CMP STORM SEWER.
- INSTALL CATCH BASIN. REFER TO DETAIL 1/C3.2.
- INSTALL 16-INCH GATE VALVE ON CITY OF BLOOMINGTON'S WATERMAIN BETWEEN WATER SERVICES. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND REGULATIONS.

# UTILITY NOTES FOR WORK IN PUBLIC RIGHT-OF-WAY:

- FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS.
- PRIOR TO CONSTRUCTION, CONTRACTORS ARE TO COORDINATE ALL WORK WITHIN RIGHT OF WAY AND OBTAIN ALL APPLICABLE PERMITS.

# UTILITY NOTES:

- COORDINATE SERVICE CONNECTION LOCATIONS AT THE BUILDING WITH THE MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR UNCOORDINATED WORK.
- ALL SEWER SERVICE CONNECTIONS WITH LESS THAN 9 FEET OF COVER OVER THE TOP OF PIPE ARE TO BE INSULATED. INSULATION SHALL BE INSTALLED FROM THE CONNECTION OF THE SERVICE AT THE BUILDING TO THE POINT WHICH THE SERVICE ATTAINS 5 FEET OF COVER. CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM ARCHITECT OR ENGINEER PRIOR TO INSTALLATION OF INSULATION.
- PROTECT ALL EXISTING STRUCTURES AND UTILITIES WHICH ARE NOT SCHEDULED TO BE REMOVED.
- ALL SEWER AND WATER CROSSINGS SHALL HAVE A MINIMUM VERTICAL SEPARATION OF 1.5 FEET AND HORIZONTAL SEPARATION OF 10 FEET FOLLOW ALL HEALTH DEPARTMENT AND CITY OF BLOOMINGTON AND HENNEPIN COUNTY STANDARDS.
- ALL WATER MAINS SHALL BE DUCTILE IRON PIPE, CLASS 52, UNLESS NOTED OTHERWISE.
- ALL WATER MAIN SHALL HAVE A MINIMUM DEPTH OF 8 FEET AND A MAXIMUM DEPTH OF 10 FEET OF COVER OVER TOP OF WATER MAIN, VALVES, SERVICES, ETC. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS.
- PROVIDE THRUST BLOCKING ON ALL WATER MAIN PER CITY OF BLOOMINGTON. PROVIDE MECHANICAL JOINT RESTRAINTS ON ALL BENDS, VALVES, TEES, PLUGS AND HYDRANT LEADS.
- SANITARY SEWER PIPING SHALL BE SDR 35 PVC UNLESS NOTED OTHERWISE.
- STORM SEWER PIPING SHALL BE SMOOTH INTERIOR AND ANNUAL EXTERIOR CORRUGATED HIGH DENSITY POLYETHYLENE (HDPE), UNLESS NOTED OTHERWISE. HOPE PIPE SHALL CONFORM TO ASTM F2306.
- CONTRACTORS ARE TO COORDINATE ALL WORK WITH GAS, ELECTRIC, TELEVISION AND TELEPHONE COMPANIES PRIOR TO START OF CONSTRUCTION.
- ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10-FEET OF THE BUILDING OR WATER SERVICE LINE SHALL BE TESTED IN ACCORDANCE WITH MN PLUMBING CODE.
- ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE GAS TIGHT OR WATER TIGHT IN ACCORDANCE TO MN PLUMBING CODE. APPROVED RESIDENT RUBBER JOINTS MUST BE USED TO MAKE WATER TIGHT CONNECTIONS TO MANHOLES, CATCH BASINS, AND OTHER STRUCTURES. GROUT RINGS ARE AN ACCEPTABLE ALTERNATIVE. CEMENT MORTAR JOINTS ARE PERMITTED ONLY FOR REPAIRS AND CONNECTIONS OF EXISTING LINES CONSTRUCTED WITH SUCH JOINTS.

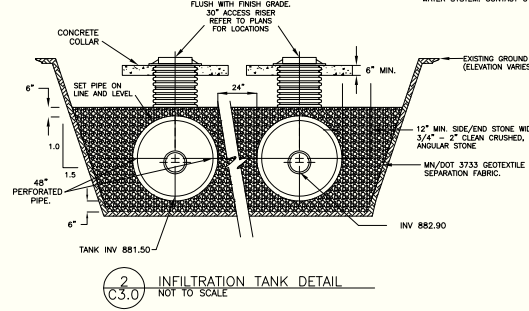
# CITY OF BLOOMINGTON NOTES:

- UTILITY AS-BUILTS MUST BE PROVIDED PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY.
- CONTRACTOR SHALL OBTAIN A PUBLIC WORKS PERMIT FOR OBSTRUCTIONS AND CONCRETE WORK WITHIN THE RIGHT-OF-WAY. PERMIT IS REQUIRED PRIOR TO REMOVALS OR INSTALLATION. CONTACT BRIAN HANSEN (952-563-4543, BHANSEN@BLOOMINGTONMN.GOV) FOR PERMIT INFORMATION.
- CONTRACTOR SHALL OBTAIN A PUBLIC WORKS PERMIT FOR UNDERGROUND WORK WITHIN THE RIGHT-OF-WAY. PERMIT IS REQUIRED PRIOR TO REMOVALS OR INSTALLATION. CONTACT UTILITIES (952-563-4548) FOR PERMIT INFORMATION.
- TEMPORARY STREET SIGNS, LIGHTING, AND ADDRESS SHALL BE PROVIDED DURING CONSTRUCTION.
- ALL CONSTRUCTION AND POST-CONSTRUCTION PARKING AND STORAGE OF EQUIPMENT AND MATERIALS MUST BE ON-SITE. USE OF PUBLIC STREETS OR PUBLIC RIGHT-OF-WAY FOR PRIVATE CONSTRUCTION PARKING, LOADING/UNLOADING, AND STORAGE WILL NOT BE ALLOWED.
- ALL PUBLIC SIDEWALKS SHALL NOT BE OBSTRUCTED.
- USE CLASS 52 DIP WATER MAIN FOR PIPE 12-INCHES IN DIAMETER AND SMALLER. A MINIMUM 8 MIL POLYWRAP IS REQUIRED ON ALL DIP.
- TAPS OF LIVE WATER MAINS ARE DONE BY CITY FORCES AND PAID FOR AND COORDINATED WITH THE CONTRACTOR.
- UTILITY AND MECHANICAL CONTRACTORS SHALL COORDINATE INSTALLATION OF WATER SERVICE PIPES, FITTINGS, AND VALVES ALL THE WAY INTO THE BUILDING (I.E. UP TO METERS AND/OR FIRE SERVICE EQUIPMENT) TO ACCOMMODATE CITY INSPECTION AND TESTING. UTILITY AND MECHANICAL CONTRACTORS SHALL COORDINATE INSTALLATION OF SEWER SERVICES ALL THE WAY INTO THE BUILDING TO ACCOMMODATE CITY INSPECTION AND TESTING.
- COMBINATION FIRE AND DOMESTIC SERVICES MUST TERMINATE WITH A THREAD ON FLANGE OR A MJ TO FLANGE ADAPTER.
- ALL COMPONENTS OF THE WATER SYSTEM, UP TO THE WATER METER OR FIRE SERVICE EQUIPMENT MUST UTILIZE PROTECTIVE INTERNAL COATINGS MEETING CURRENT AND/FUTURE STANDARDS FOR CEMENT MORTAR LINING OR SPECIAL COATINGS. THE USE OF UNLINED OR UNCOATED PIPE IS NOT ALLOWED.
- UTILITY PERMITS ARE REQUIRED FOR CONNECTIONS TO THE PUBLIC STORM, SANITARY, AND WATER SYSTEM. CONTACT UTILITIES (952-563-8777) FOR PERMIT INFORMATION.

1 C3.0 UTILITY PLAN 1"=30'

# STORM SEWER TABLE

| STRUCTURE IDENTIFICATION | STRUCTURE DIMENSION (INCHES) | NEENAH CASTING | TOP ELEVATION | INVERT ELEVATION  | PIPE LENGTH, DIAMETER, SLOPE & NEXT UPSTREAM STRUCTURE                             |
|--------------------------|------------------------------|----------------|---------------|---|--|
| EX. CB #1                | NA                           | NA             | 885.60        | S. 882.70<br>N. 882.70                                  | 22 L.F. OF 12" HDPE PIPE @ 1.00%, TREATMENT TANK                                   |
| CB #2                    | 48                           | R-3067         | 885.90        | W. 882.45<br>SUMP = 879.45                              |  |
| CB #3                    | 48                           | R-3067         | 885.50        | E. 884.95<br>W. 884.95<br>SUMP = 881.95                 | 81 L.F. OF 12" RCP @ 1.00%, CB #4  |
| CB #4                    | 48                           | R-3067         | 886.70        | E. 885.76   |  |
| TREATMENT TANK           | NA                           | NA             | NA            | S. 882.90<br>E. 882.20<br>W. 883.50<br>INV. TANK 881.50 | 25 L.F. OF 12" HDPE PIPE @ 1.00%, CB #2<br>29 L.F. OF 12" HDPE PIPE @ 5.00%, CB #3 |
| MH #5                    | 48                           |                | 886.90        | S. 882.50<br>NW. 882.50                                 | 31 L.F. OF 12" CMP @ 1.00%, TREATMENT TANK   |



- UTILIZE CARE WHEN LOWERING UNIT INTO THE TRENCH. HANDLE USING NYLON SLINGS AND TWO PICK POINTS.
- PLACE BACKFILL AROUND THE UNIT IN UNIFORM 8"-12" LIFTS, COMPACTED TO 90% SD.
- WHEN THE UNIT CONSISTS OF 2 SECTIONS, PLACE THE DOWNSTREAM SECTION FIRST. CONNECT AND HOME THE REMAINING SECTIONS.
- ALL CONNECTION POINTS ARE TO BE SOIL PROOF. ACCESS RISERS AND CONCRETE COLLARS SHALL BE DESIGNED TO WITHSTAND HS25 LOADING.

|                       |                   |
|-----------------------|-------------------|
| ROW LENGTH:           | 100 FEET          |
| NUMBER OF ROWS:       | 7                 |
| NUMBER OF HEADERS:    | 2                 |
| INFILTRATION VOLUME:  | 5,488 CUBIC FEET  |
| TOTAL STORAGE VOLUME: | 16,200 CUBIC FEET |

Peace Lutheran Church

8600 E. Bush Lake Rd. Bloomington, MN 55438

BKBM 8600 Brooklyn Boulevard Bloomington, MN 55438-0818 Phone (952) 563-4543 Fax (952) 563-4544 www.bkblm.com

© 2014 BKBM Engineers, Inc. All rights reserved. This document is an instrument of service and is the property of BKBM. It contains confidential information and is not to be reproduced or used for any other project without prior written consent. BKM PROJECT NO. 16359.0

Project Status

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota

19181

Joel Maier Registration No.

SEAL

DATE 8/24/2016

PROJ. NO. 16359.00

UTILITY PLAN

TITLE

C3.0

SHEET NO.



W. 86TH ST.

CO. RD NO. 28  
(EAST BUSH LAKE ROAD)

| PROPOSED PLAN SYMBOLS            |     |
|----------------------------------|-----|
| CONSTRUCTION LIMITS              | --- |
| PROPERTY LINE                    | --- |
| SAWCUT LINE (APPROX.)            | --- |
| RETAINING WALL                   | --- |
| MEDIUM-DUTY BITUMINOUS PAVEMENT  | --- |
| CONCRETE PAVEMENT/SIDWALK        | --- |
| BLOOMINGTON CITY STREET PAVEMENT | --- |
| FLAT CURB AND GUTTER             | --- |
| PEDESTRIAN CURB RAMP             | --- |
| ACCESSIBLE PARKING SYMBOL        | --- |
| SIGN                             | --- |
| PARKING STALL COUNT              | --- |

NOTE: CONSTRUCTION LIMITS ARE ANTICIPATED TO BE PROPERTY LINE UNLESS OTHERWISE SHOWN.

KEYED NOTES

- KEYED NOTES ARE DENOTED BY [X] ON PLAN.
- INSTALL B612 CONCRETE CURB AND GUTTER. REFER TO DETAIL 7/C5.1
  - REPAIR STREET BY COMPLYING WITH BLOOMINGTON'S CITY STREET IMPROVEMENT POLICY. REFER TO DETAIL 2/C5.2.
  - INSTALL LIGHT-DUTY BITUMINOUS PAVEMENT. REFER TO DETAIL 9/C5.1.
  - INSTALL CONCRETE WALK. REFER TO DETAIL 11/C5.1, REFER TO LANDSCAPE PLANS FOR CONCRETE FINISH AND JOINTING.
  - INSTALL PEDESTRIAN CURB RAMP. REFER TO DETAIL 2/C5.0. REFER TO MVDOT STANDARD PLAN 5-297.250 AND MN/DOT STANDARD PLATE 703B.
  - INSTALL ACCESSIBLE PARKING AND NO PARKING SIGNS. REFER TO DETAIL 5/C5.1.
  - INSTALL NONRESIDENTIAL CONCRETE DRIVEWAY APRON. REFER TO DETAIL 1/C5.1.
  - INSTALL B618 CURB AND GUTTER IN THE RIGHT-OF-WAY. REFER TO DETAIL 2/C5.1.
  - INSTALL FLAT CONCRETE CURB. REFER TO DETAIL 6/C5.1.
  - INSTALL MEDIUM-DUTY BITUMINOUS PAVEMENT. REFER TO DETAIL 10/C5.1.
  - PROOF OF PARKING.

| ABBREVIATIONS |                          |
|---------------|--------------------------|
| BUDG          | Building                 |
| BM            | Bench Mark               |
| BM            | Bottom of Wall           |
| CONC          | Concrete                 |
| ELEV          | Elevation                |
| EX            | Existing                 |
| FTE           | Finished Floor Elevation |
| MAX           | Maximum                  |
| MIN           | Minimum                  |
| R             | Radius                   |
| TW            | Top of Wall              |
| TYP.          | Typical                  |

GENERAL NOTES:

- ALL DIMENSIONS ARE TO FACE OF CURB UNLESS NOTED OTHERWISE.
- ALL CURB AND GUTTER IS TO BE B612 CONCRETE CURB AND GUTTER UNLESS NOTED OTHERWISE.
- NO SIDEWALK IS TO HAVE MORE THAN A 2% CROSS SLOPE OR MORE THAN A 5% LONGITUDINAL SLOPE.
- REFER TO ARCHITECTURAL PLANS FOR PROPOSED BUILDING LAYOUT.
- FOLLOW ALL CITY OF BLOOMINGTON AND HENNEPIN COUNTY RULES, REGULATIONS AND SPECIFICATIONS WHEN WORKING IN PUBLIC RIGHT OF WAY.
- NO PONDING OF WATER OR ABRUPT TRANSITIONS WILL BE ALLOWED WHERE NEW PAVEMENT/CURB/SIDWALK MATCHES INTO EXISTING PAVEMENT/CURB/SIDWALK.
- ALL PARKING STALLS, EXCEPT ACCESSIBLE STALLS, ARE TO BE 9 FEET WIDE BY 20 FEET LONG.
- THE CONTRACTOR IS TO CONTACT THE CITY OF BLOOMINGTON FIRE MARSHALL FOR THE EXACT PLACEMENT OF FIRE LANES, YELLOW-PAINTED CURBING AND NO PARKING AREAS FOR FIRE PROTECTION PURPOSES.

PAVING NOTES:

- REFER TO STRUCTURAL PLANS FOR STOOPT DETAILS. ALL WALKS ARE TO BE CENTERED ON THE DOORS.
- INSTALL APPROPRIATE EXPANSION MATERIAL WHERE CONCRETE IS ADJACENT TO BUILDING FACE.
- SAWCUT AND MATCH NEW BITUMINOUS PAVEMENT INTO EXISTING PAVEMENT. NO ABRUPT GRADE TRANSITIONS OR PONDING OF WATER WILL BE ALLOWED.
- MATCH NEW CONCRETE CURB AND GUTTER INTO EXISTING. FOLLOW ALL CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS FOR CURB TYPE, MATERIAL, AND INSTALLATION METHODS.
- SAWCUT EXISTING BITUMINOUS PAVEMENT AND CURB AND GUTTER TO NEAREST JOINT. COORDINATE REMOVAL LIMITS WITH SITE DEMOLITION CONTRACTOR AND CONSTRUCTION MANAGER. INSTALL DRIVE ENTRANCE FOR CITY OF BLOOMINGTON STANDARDS AND SPECIFICATIONS. FOLLOW ALL CITY OF BLOOMINGTON AND HENNEPIN COUNTY REQUIREMENTS FOR TRAFFIC CONTROL.

CITY OF BLOOMINGTON NOTES:

- UTILITY AS-BUILTS MUST BE PROVIDED PRIOR TO ISSUANCE OF CERTIFICATE OF OCCUPANCY.
- CONTRACTOR SHALL OBTAIN A PUBLIC WORKS PERMIT FOR OBSTRUCTIONS AND CONCRETE WORK WITHIN THE RIGHT-OF-WAY. PERMIT IS REQUIRED PRIOR TO REMOVALS OR INSTALLATION. CONTACT BRIAN HANSEN (952-563-4543, BHANSEN@BLOOMINGTONMN.GOV) FOR PERMIT INFORMATION.
- CONTRACTOR SHALL OBTAIN A PUBLIC WORKS PERMIT FOR UNDERGROUND WORK WITHIN THE RIGHT-OF-WAY. PERMIT IS REQUIRED PRIOR TO REMOVALS OR INSTALLATION. CONTACT UTILITIES (952-563-4568) FOR PERMIT INFORMATION.
- TEMPORARY STREET SIGNS, LIGHTING, AND ADDRESSES SHALL BE PROVIDED DURING CONSTRUCTION.
- ALL CONSTRUCTION AND POST-CONSTRUCTION PARKING AND STORAGE OF EQUIPMENT AND MATERIALS MUST BE ON-SITE. USE OF PUBLIC STREETS AND PUBLIC RIGHT-OF-WAY FOR PRIVATE CONSTRUCTION PARKING, LOADING/UNLOADING, AND STORAGE WILL NOT BE ALLOWED.
- ALL PUBLIC SIDEWALKS SHALL NOT BE OBSTRUCTED.
- USE CLASS 52 DIP WATER MAIN FOR PIPE 12-INCHES IN DIAMETER AND SMALLER. A MINIMUM 8 MIL POLYWRAP IS REQUIRED ON ALL DIP.
- TAPS OF LIVE WATER MAINS ARE DONE BY CITY FORCES AND PAID FOR AND COORDINATED WITH THE CONTRACTOR.
- UTILITY AND MECHANICAL CONTRACTORS SHALL COORDINATE INSTALLATION OF WATER SERVICE PIPES, FITTINGS, AND VALVES ALL THE WAY INTO THE BUILDING (I.E. UP TO METERS AND/OR FIRE SERVICE EQUIPMENT) TO ACCOMMODATE CITY INSPECTION AND TESTING. UTILITY AND MECHANICAL CONTRACTORS SHALL COORDINATE INSTALLATION OF SEWER SERVICES ALL THE WAY INTO THE BUILDING TO ACCOMMODATE CITY INSPECTION AND TESTING.
- COMBINATION FIRE AND DOMESTIC SERVICES MUST TERMINATE WITH A THREAD ON FLANGE OR A MI TO FLANGE ADAPTER.
- ALL COMPONENTS OF THE WATER SYSTEM, UP TO THE WATER METER OR FIRE SERVICE EQUIPMENT MUST UTILIZE PROTECTIVE INTERNAL COATINGS MEETING CURRENT AND/OR AWWA STANDARDS FOR CEMENT MORTAR LINING OR SPECIAL COATINGS. THE USE OF UNJOINED OR UNCOATED PIPE IS NOT ALLOWED.
- UTILITY PERMITS ARE REQUIRED FOR CONNECTIONS TO THE PUBLIC STORM, SANITARY, AND WATER SYSTEM. CONTACT UTILITIES (952-563-8777) FOR PERMIT INFORMATION.
- ALL PARKING STALL STRIPING MUST BE PAINTED WHITE.

Peace Lutheran Church

8600 E. Bush Lake Rd. Bloomington, MN 55438

BKBM  
2020 Brooklyn Boulevard  
Bloomington, MN 55405  
Phone (952) 563-4543  
Fax (952) 563-4543  
www.bkbm.com

© 2018 BKBM Systems, Inc.  
All rights reserved.  
This document is an instrument of service and is the property of BKBM.  
It remains the property of BKBM until it is returned to BKBM in its entirety.  
If returned, it may not be used or reproduced without prior written consent.  
BKBM PROJECT NO. 19181

Project Status

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota

Joel Maier Registration No. 19181

SEAL

DATE 8/24/2016

PROJ. NO. 16359.00

PAVING AND GEOMETRIC PLAN

TITLE

C4.0

SHEET NO.



PROJECT  
**Peace Lutheran Church**

CLIENT  
**Peace Lutheran Church**

ARCHITECT  
**Alliance**  
612.874.4100

LANDSCAPE ARCHITECT  
**Calyx Design Group**  
651.788.9018

Civil/STRUCTURAL ENGINEER  
**BKBM Engineers**  
763.843.0420

MECHANICAL/ELECTRICAL/  
PLUMBING ENGINEERS  
**Emanuelson Podas**  
952.930.0050

Food Service Consultant  
**Rippe & Associates**  
952.933.0313

I hereby certify that this plan or drawing was prepared by me  
or under my direct supervision and that I am a duly licensed  
under the laws of the State of Minnesota.

FOR  
BY  
NAME  
DATE  
REG. NO.

Issue Date

ISSUED FOR  
PLANNING COMMISSION SUBMISSION

DATE

08/24/2016

Copyright © Alliance

COMMISSION NO.

Author

DRAWN

Checker

CHECKED

DATE

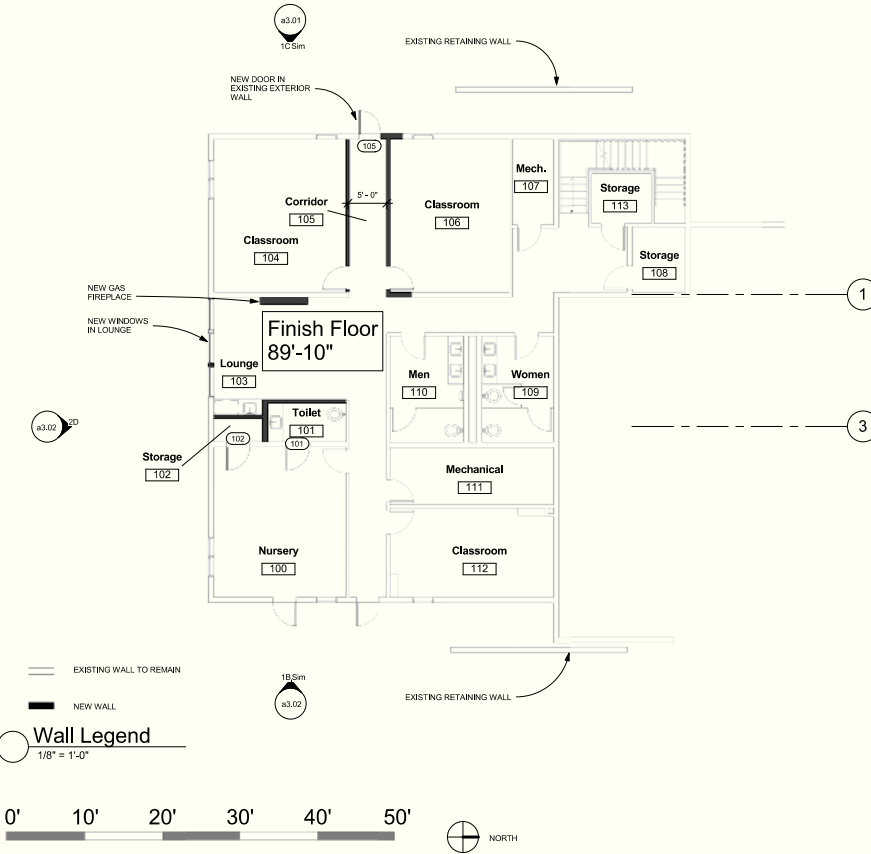
08/24/16

**PRELIMINARY**  
NOT FOR CONSTRUCTION

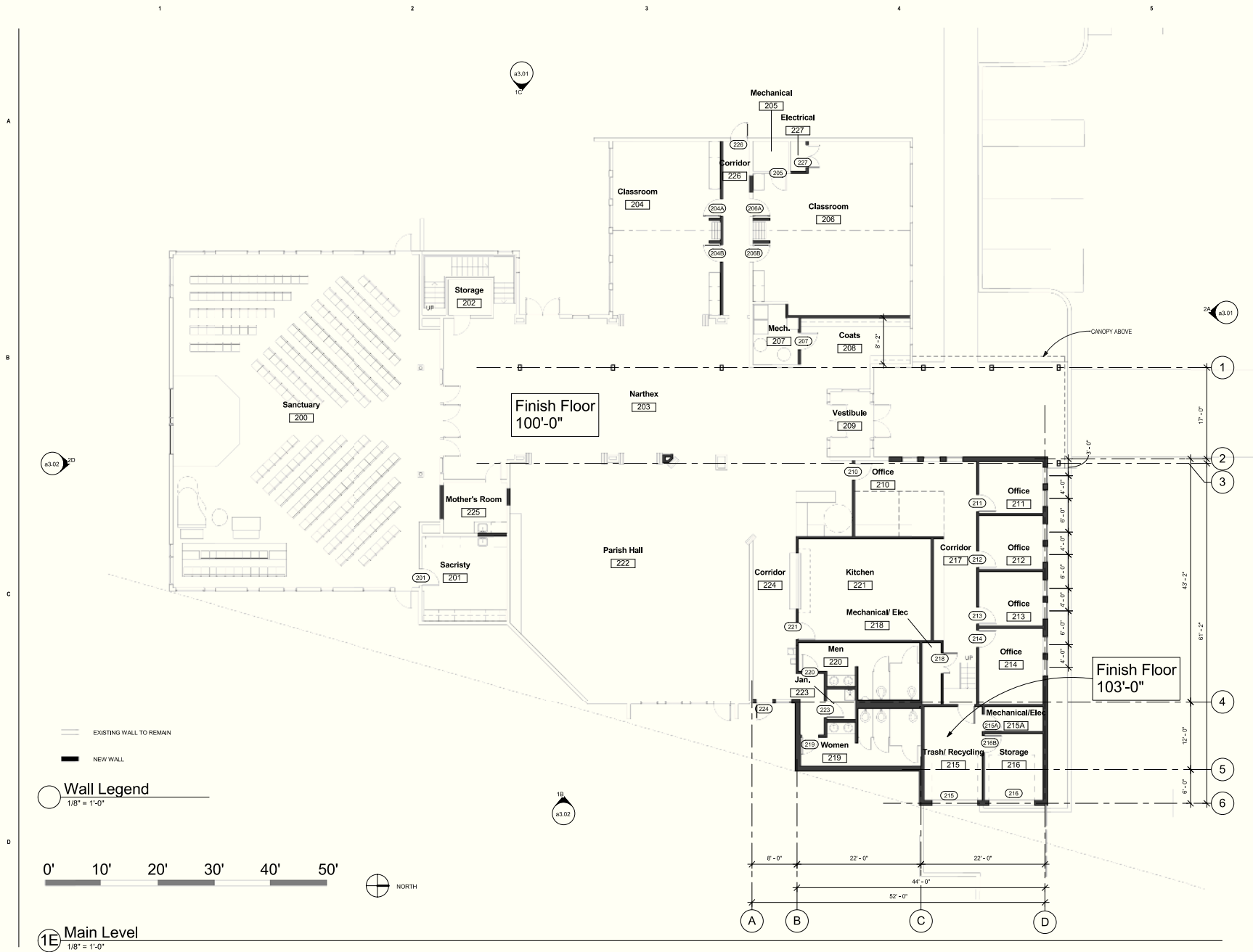
**ALLIANCE**

Lower Level Floor  
Plan

SHEET  
**a1.B1**



**1E Lower Level Floor Plan**  
1/8" = 1'-0"



PROJECT

**Peace Lutheran Church**

CLIENT

Peace Lutheran Church

ARCHITECT

**Alliance**

612.874.4100

LANDSCAPE ARCHITECT

**Calyx Design Group**

651.788.9018

Civil/Structural Engineer

**BKBM Engineers**

763.843.0420

MECHANICAL/ELECTRICAL/PLUMBING ENGINEERS

**Emanuelson Podas**

952.930.0050

Food Service Consultant

**Rippe & Associates**

952.933.0313

I hereby certify that this plan or drawing was prepared by me or under my direct supervision and that I am a duly licensed under the laws of the State of Minnesota.

FOR

BY

NAME

DATE

REG. NO.

Issue Date

ISSUED FOR

PLANNING COMMISSION SUBMISSION

DATE

08/24/2018

Copyright © Alliance

COMMISSION NO.

DRAWN

CHECKED

DATE

Author

Checker

08/24/16

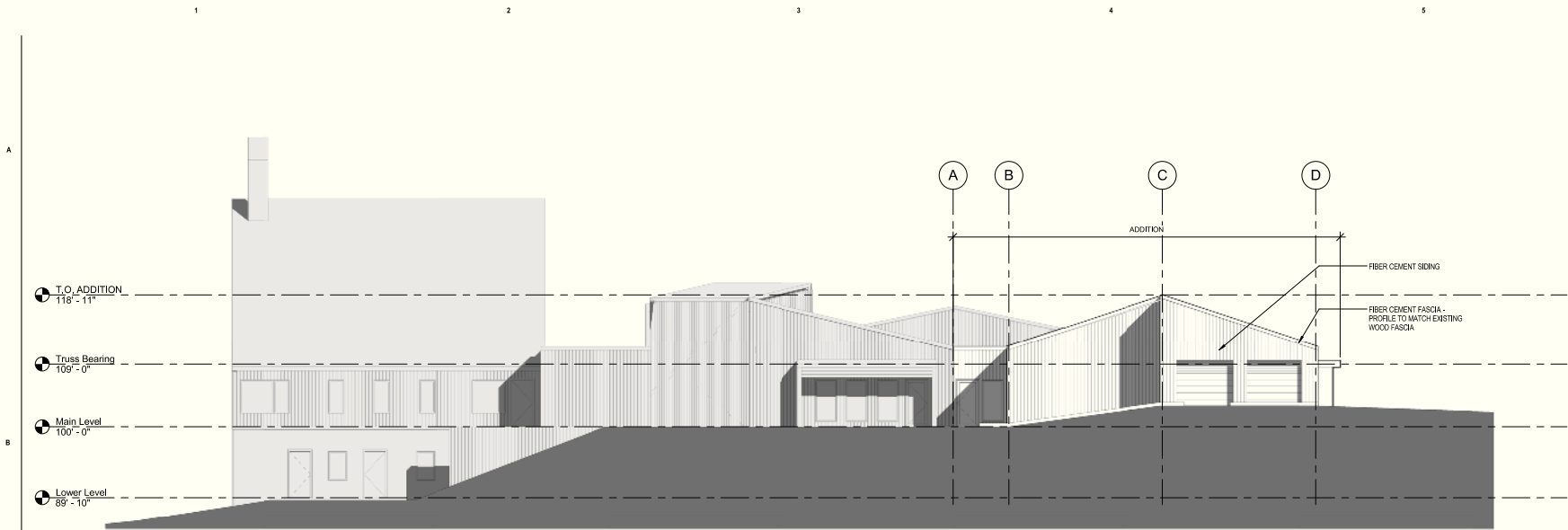
**PRELIMINARY**  
NOT FOR CONSTRUCTION

**ALLIANCE**

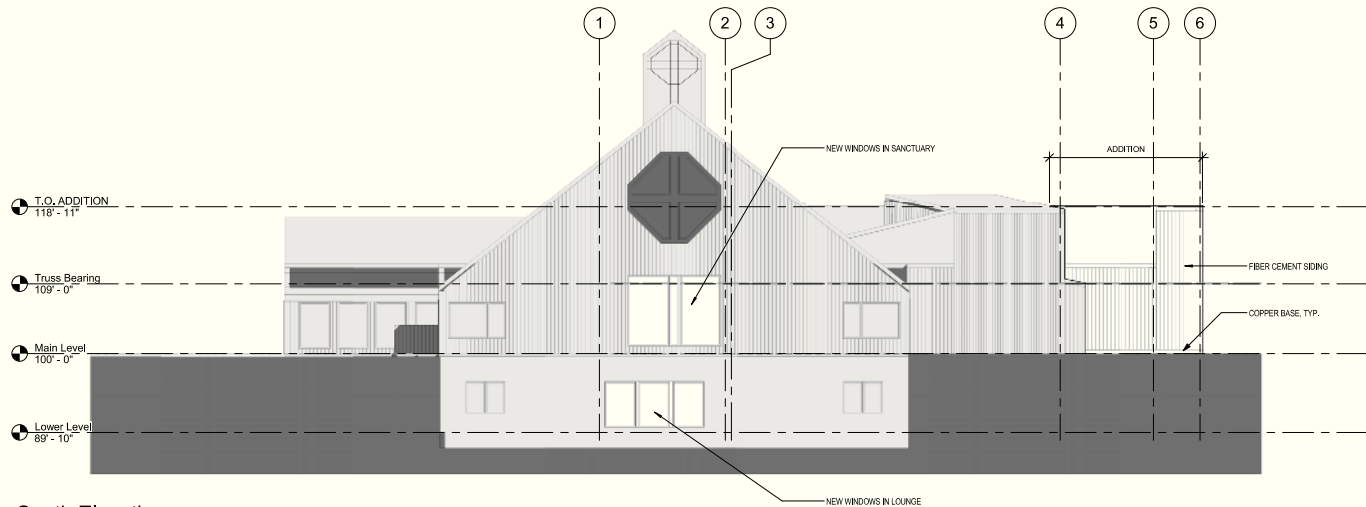
Main Level Floor Plan

**a1.01**





1B East Elevation  
1/8" = 1'-0"



2D South Elevation  
1/8" = 1'-0"

PROJECT

**Peace Lutheran Church**

CLIENT

Peace Lutheran Church

ARCHITECT

**Alliance**

612.874.4100

LANDSCAPE ARCHITECT

**Calyx Design Group**

651.788.9018

Civil/Structural Engineer

**BKBM Engineers**

763.843.0420

MECHANICAL, ELECTRICAL,  
PLUMBING ENGINEERS

**Emanuelson Podas**

952.930.0050

Food Service Consultant

**Rippe & Associates**

952.933.0313

I hereby certify that this plan or drawing was prepared by me or under my direct supervision and that I am a duly licensed under the laws of the State of Minnesota.

FOR

BY

NAME

DATE

REG. NO.

Issue Date

ISSUED FOR

PLANNING COMMISSION SUBMISSION

DATE

08/24/2016

Copyright © Alliance

COMMISSION NO.

DRAWN

CHECKED

DATE

Author

Checker

08/24/16

**PRELIMINARY**  
NOT FOR CONSTRUCTION

**ALLIANCE**

Exterior Elevations

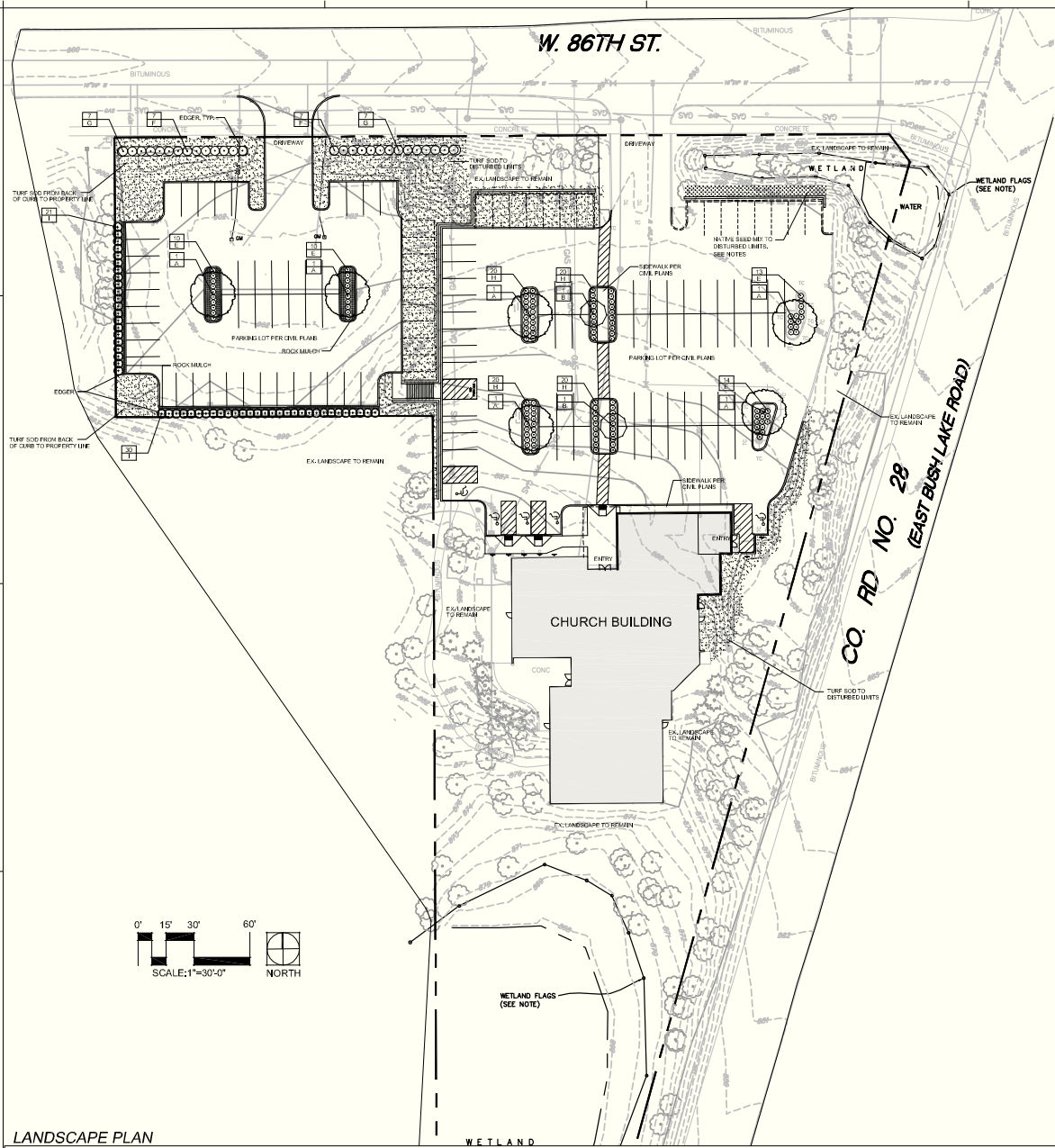
**a3.02**

SHEET


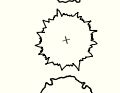
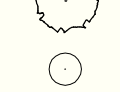
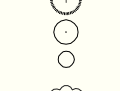
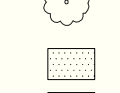
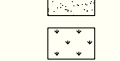
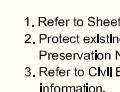
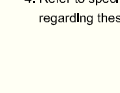





SHEET NO. \_\_\_\_\_





#### Planting Symbols Legend:

-  Proposed Deciduous Overstory Tree
-  Proposed Coniferous Tree
-  Proposed Deciduous Ornamental Tree
-  Proposed Large Deciduous Shrub
-  Proposed Large Coniferous Shrub
-  Proposed Medium Deciduous Shrub
-  Proposed Perennial Plant
-  Existing Tree to Remain  
(See Civil Plans for Tree Removals)
-  Rock Mulch
-  Turf Sod
-  Native Seed Mix

#### Notes:

1. Refer to Sheet L102 for Details, Notes, and Requirements.
2. Protect existing trees to remain per Sheet L100 and Tree Preservation Notes
3. Refer to Civil Engineer's plans for grading, drainage, and site plan information.
4. Refer to specifications manual for additional requirements regarding these plans.



**CALYX DESIGN GROUP**  
Landscape Architecture  
Sustainable Design  
Master Planning

370 Selby Avenue | Suite 301  
Saint Paul, MN 55102  
Telephone: 651.788.5018  
Internet: www.calyxdesigngroup.com

**Peace Lutheran Church**

8600 E. Bush Lake Rd. Bloomington, MN 55438

Project Status

**PRELIMINARY  
NOT FOR  
CONSTRUCTION**

PLANNING COMMISSION  
SUBMISSION 08.24.16

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed LANDSCAPE ARCHITECT under the laws of the State of MINNESOTA

48084  
Ben Harberg Registration No.

SEAL

DATE 8/24/2016

PROJ. NO. 1511.00

**LANDSCAPE  
PLAN**

TITLE

**L101**

SHEET NO.

LANDSCAPE PLAN



Landscape Notes & Requirements:

1. Tree saucer for individual trees outside of a plant bed to be four inches (4") deep radial slope-tined hardwood mulch for trees outside of a plant bed. Install per tree planting detail. Do not place mulch against tree trunk. Remove wire and buffer from top 24" of root ball before final soil back-fill and mulch.
2. Refer to drill plan sheets for grading, drainage, site dimensions, survey, tree removal, proposed utilities & erosion control.
3. All plant material must comply with the latest edition of the American Standards for Nursery, American Association of Nurserymen. Unless noted otherwise, deciduous shrubs shall have 4-6 inch 8 corner at the specified shrub height. Plant material shall be delivered as specified.
4. Plant takes precedence over plant schedule if discrepancies in quantities exist.
5. All proposed plants shall be located and scaled as shown.
6. Adjustment in location of proposed plant material may be needed in field. Should an adjustment be required, the client will provide field approval. Significant changes may require city review and approval.
7. The project landscape contractor shall be held responsible for weeding and properly handling all plant materials brought on the site both before and after installation. Schedule plant deliveries to coincide with expected installation time within 30 hours.
8. All plant materials shall be fielded upon installation as specified.
9. The landscape contractor shall provide the owner with a watering schedule appropriate to the project site conditions and to plant material growth requirements.
10. If the landscape contractor is concerned or perceives any deficiencies in the plant selections, soil conditions, drainage or any other site condition that might negatively affect plant establishment, survival or guarantee, they must bring these deficiencies to the attention of the landscape architect & client prior to bid submission. Plant bed drainage concerns during plant installation shall be brought to the attention of the Owner and General Contractor immediately.
11. Contractor shall establish to his/her satisfaction that soil and compaction conditions are adequate to allow for proper drainage at and around the building site.
12. Contractor is responsible for anything maintenance of all newly installed material until time of owner acceptance. Any acts of vandalism or damage which may occur prior to owner acceptance shall be the responsibility of the contractor. Contractor shall provide the owner with a maintenance program including, but not limited to, pruning, fertilization and disease/pest control.
13. The contractor shall guarantee newly planted material through one calendar year from the date of written owner acceptance. Plants that exhibit more than 10% dieback damage shall be replaced at no additional cost to the owner. The contractor shall also provide adequate weed and deer/moose protection measures for the plantings during the warranty period.
14. This layout plan constitutes our understanding of the landscape requirements listed in the ordinance. Changes and modifications may be requested by the city based on applicant information, public input, council decisions, etc.
15. The landscape contractor shall be responsible for obtaining any permits and coordinating inspections as required throughout the work process.
16. Plant size & species substitutions must be approved in writing prior to acceptance in the field.
17. The landscape contractor shall furnish an Irrigation Layout Plan for head-to-head coverage of all tree, turf and shrub planting areas. Use Hunter commercial-grade irrigation equipment and provide product cut-sheets and (3) copies of the proposed layout plan to the landscape architect for review and approval prior to installation. Coordinate irrigation scheduling under pavements, connector paths, corridors, beds-lawn, and various locations with the mechanical engineer, architect, & general contractor. Include 1 full shutdown and spring start-up in Irrigation bid.
18. All edges shall be professional grade black steel edger, 1/8" thickness. Anchor every 18" on-center (minimum). Submit sample.
19. Landscape Contractor is responsible for coordination with the General Contractor, to protect the new improvements on and off-site during landscape work activities. Report any damage to the General Contractor immediately.
20. Unless otherwise noted indicated, plant beds shall receive rock mulch over fabric weed mat, per detail. Submit mulch sample for Owner approval. Do not install weed mat under perennials, with the exception of ornamental grasses.
21. Rock mulch areas (all plant beds) shall be 1.5"-2" inch dia. washed limestone rock. Install per detail. Submit mulch sample for approval.
22. All planting, seeding, and sodded areas shall be prepared prior to installation activities with a harley power box rake or equal to provide a firm planting bed free of stones, sods, construction debris, etc.
23. Turf Seeding activity shall conform to all rules and regulations as established in the MnDOT Seeding Manual, 2014 edition, for seed bed preparation, installation, maintenance, accessibility, and warranty.
24. The Landscape Contractor shall furnish samples of all landscape materials for approval prior to installation.
25. The Landscape Contractor shall clean and grub the underbrush from within the work limits to remove dead branches, leaves, trash, weeds and foreign materials.
26. The landscape contractor shall contact Gopher State One Call no less than 48 hours before digging for field utility locations.
27. The landscape contractor shall be responsible for the removal of erosion control measures once vegetation has been established to the satisfaction of the municipal staff. This includes all erosion fencing and sediment logs placed in the landscape.
28. The landscape contractor shall be responsible for visiting the site to become familiar with the conditions prior to bidding and installation. Coordinate with the general contractor on means such as the grading, landscape area conditions, staging areas, irrigation connection to building, etc.
29. See Site and Civil plans for additional information regarding the project, including irrigation area soils and sub-surface drainage requirements and performance.
30. Topsoil Requirements: All graded areas of the site that are designated on the plan set for top soil shall have no less than 6" of imported top soil, areas designated for shrubs, trees, and perennials shall have no less than 12" of imported top soil, meeting MnDOT classifications for planting soil for trees, shrubs, and turf. Slope away from building.
31. Landscape contractor must prove the open sub-grade of all planting areas after their excavation is capable of holding a minimum requirement of 1.0 inch of water per hour prior to installation of plant materials, topsoil, irrigation, weed mat, and mulch. Planting areas not capable of meeting this requirement shall have 4" diameter x 48" depth holes augured every 36" on-center and filled with MnDOT Free-Draining Coarse Fiber Aggregate. Re-test sub-grade penetration for compliance to infiltration minimum requirement.
32. Trees marked for removal on civil plans shall be cut down with the stump ground to 30" below grade. Remove all tree debris and dispose of off-site. No splitting or burning on-site will be allowed. Obtain permit for removal of trees in the right-of-way from the City of Bloomington. See Civil Plan for trees to be removed.
33. Landscape contractor to provide nursery pull list (bill of lading) including plant species and sizes shipped to the site. Additionally, the landscape contractor shall provide nursery book inventory, proving none of the materials provided consist of any genetic strains of the monocotyledon family including aspen, birch, hickory, hickory, maple, Norway spruce, redwood, and yew.
34. All trees must be field located and reviewed by the City of Bloomington prior to installation, to ensure a minimum 6 foot separation from public utilities.

Landscape Plant List:

| TREES | Qty | Key | Scientific Name                     | Common Name            | Size     | Root | Notes |
|-------|-----|-----|-------------------------------------|------------------------|----------|------|-------|
|       |     |     |                                     |                        |          |      |       |
| 1     | 5   | A   | Thuja occidentalis                  | Landlord Lavender      | 3.0' Cal | B&B  |       |
|       | 2   | B   | Crataegus laevigata 'Crimson Cloud' | Crimson Cloud Hawthorn | 2.0' Cal | B&B  |       |

| SHRUBS | Qty | Key | Scientific Name                          | Common Name             | Size | Root | Notes |
|--------|-----|-----|--|-------------------------|------|------|-------|
|        |     |     |  |                         |      |      |       |
| 47     | 1   | E   | Doronicum laetare                        | Dwarf Bush Honey-suckle | 42   | POT  |       |
|        | 14  | F   | Cornus sericea 'Scent'                   | White Dogwood           | 45   | POT  |       |
| 14     | G   | G   | Syringa patula 'Miss Kim'                | Miss Kim Lilac          | 45   | POT  |       |
|        | 10  | L   | Calamagrostis canadensis 'Karl Foerster' | Feather Reed Grass      | 42   | POT  |       |
| 11     | L   | L   | Thalictrum aquilegifolium 'Tudor Globe'  | Tudor Globe Anemone     | 45   | POT  |       |

SEEDING SPECIFICATIONS:

|   |  |
|---|--|
|  | Woodland Edge Central Seed Mix, MnDOT 4 36-711 |
|---|--|

| Plant                   | Quantity | Unit | Price | Total | Plant                   | Quantity | Unit | Price | Total |
|-------------------------|----------|------|-------|-------|-------------------------|----------|------|-------|-------|
| 1. American Elm         | 1        | each | 1.00  | 1.00  | 11. Red-twigged Dogwood | 1        | each | 1.00  | 1.00  |
| 2. White Birch          | 1        | each | 1.00  | 1.00  | 12. Black Cherry        | 1        | each | 1.00  | 1.00  |
| 3. Norway Spruce        | 1        | each | 1.00  | 1.00  | 13. Green Ash           | 1        | each | 1.00  | 1.00  |
| 4. White Pine           | 1        | each | 1.00  | 1.00  | 14. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 5. Eastern White Pine   | 1        | each | 1.00  | 1.00  | 15. White Birch         | 1        | each | 1.00  | 1.00  |
| 6. Norway Spruce        | 1        | each | 1.00  | 1.00  | 16. Green Ash           | 1        | each | 1.00  | 1.00  |
| 7. White Pine           | 1        | each | 1.00  | 1.00  | 17. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 8. Eastern White Pine   | 1        | each | 1.00  | 1.00  | 18. White Birch         | 1        | each | 1.00  | 1.00  |
| 9. Norway Spruce        | 1        | each | 1.00  | 1.00  | 19. Green Ash           | 1        | each | 1.00  | 1.00  |
| 10. White Pine          | 1        | each | 1.00  | 1.00  | 20. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 11. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 21. White Birch         | 1        | each | 1.00  | 1.00  |
| 12. Norway Spruce       | 1        | each | 1.00  | 1.00  | 22. Green Ash           | 1        | each | 1.00  | 1.00  |
| 13. White Pine          | 1        | each | 1.00  | 1.00  | 23. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 14. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 24. White Birch         | 1        | each | 1.00  | 1.00  |
| 15. Norway Spruce       | 1        | each | 1.00  | 1.00  | 25. Green Ash           | 1        | each | 1.00  | 1.00  |
| 16. White Pine          | 1        | each | 1.00  | 1.00  | 26. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 17. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 27. White Birch         | 1        | each | 1.00  | 1.00  |
| 18. Norway Spruce       | 1        | each | 1.00  | 1.00  | 28. Green Ash           | 1        | each | 1.00  | 1.00  |
| 19. White Pine          | 1        | each | 1.00  | 1.00  | 29. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 20. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 30. White Birch         | 1        | each | 1.00  | 1.00  |
| 21. Norway Spruce       | 1        | each | 1.00  | 1.00  | 31. Green Ash           | 1        | each | 1.00  | 1.00  |
| 22. White Pine          | 1        | each | 1.00  | 1.00  | 32. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 23. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 33. White Birch         | 1        | each | 1.00  | 1.00  |
| 24. Norway Spruce       | 1        | each | 1.00  | 1.00  | 34. Green Ash           | 1        | each | 1.00  | 1.00  |
| 25. White Pine          | 1        | each | 1.00  | 1.00  | 35. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 26. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 36. White Birch         | 1        | each | 1.00  | 1.00  |
| 27. Norway Spruce       | 1        | each | 1.00  | 1.00  | 37. Green Ash           | 1        | each | 1.00  | 1.00  |
| 28. White Pine          | 1        | each | 1.00  | 1.00  | 38. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 29. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 39. White Birch         | 1        | each | 1.00  | 1.00  |
| 30. Norway Spruce       | 1        | each | 1.00  | 1.00  | 40. Green Ash           | 1        | each | 1.00  | 1.00  |
| 31. White Pine          | 1        | each | 1.00  | 1.00  | 41. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 32. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 42. White Birch         | 1        | each | 1.00  | 1.00  |
| 33. Norway Spruce       | 1        | each | 1.00  | 1.00  | 43. Green Ash           | 1        | each | 1.00  | 1.00  |
| 34. White Pine          | 1        | each | 1.00  | 1.00  | 44. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 35. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 45. White Birch         | 1        | each | 1.00  | 1.00  |
| 36. Norway Spruce       | 1        | each | 1.00  | 1.00  | 46. Green Ash           | 1        | each | 1.00  | 1.00  |
| 37. White Pine          | 1        | each | 1.00  | 1.00  | 47. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 38. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 48. White Birch         | 1        | each | 1.00  | 1.00  |
| 39. Norway Spruce       | 1        | each | 1.00  | 1.00  | 49. Green Ash           | 1        | each | 1.00  | 1.00  |
| 40. White Pine          | 1        | each | 1.00  | 1.00  | 50. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 41. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 51. White Birch         | 1        | each | 1.00  | 1.00  |
| 42. Norway Spruce       | 1        | each | 1.00  | 1.00  | 52. Green Ash           | 1        | each | 1.00  | 1.00  |
| 43. White Pine          | 1        | each | 1.00  | 1.00  | 53. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 44. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 54. White Birch         | 1        | each | 1.00  | 1.00  |
| 45. Norway Spruce       | 1        | each | 1.00  | 1.00  | 55. Green Ash           | 1        | each | 1.00  | 1.00  |
| 46. White Pine          | 1        | each | 1.00  | 1.00  | 56. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 47. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 57. White Birch         | 1        | each | 1.00  | 1.00  |
| 48. Norway Spruce       | 1        | each | 1.00  | 1.00  | 58. Green Ash           | 1        | each | 1.00  | 1.00  |
| 49. White Pine          | 1        | each | 1.00  | 1.00  | 59. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 50. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 60. White Birch         | 1        | each | 1.00  | 1.00  |
| 51. Norway Spruce       | 1        | each | 1.00  | 1.00  | 61. Green Ash           | 1        | each | 1.00  | 1.00  |
| 52. White Pine          | 1        | each | 1.00  | 1.00  | 62. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 53. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 63. White Birch         | 1        | each | 1.00  | 1.00  |
| 54. Norway Spruce       | 1        | each | 1.00  | 1.00  | 64. Green Ash           | 1        | each | 1.00  | 1.00  |
| 55. White Pine          | 1        | each | 1.00  | 1.00  | 65. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 56. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 66. White Birch         | 1        | each | 1.00  | 1.00  |
| 57. Norway Spruce       | 1        | each | 1.00  | 1.00  | 67. Green Ash           | 1        | each | 1.00  | 1.00  |
| 58. White Pine          | 1        | each | 1.00  | 1.00  | 68. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 59. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 69. White Birch         | 1        | each | 1.00  | 1.00  |
| 60. Norway Spruce       | 1        | each | 1.00  | 1.00  | 70. Green Ash           | 1        | each | 1.00  | 1.00  |
| 61. White Pine          | 1        | each | 1.00  | 1.00  | 71. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 62. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 72. White Birch         | 1        | each | 1.00  | 1.00  |
| 63. Norway Spruce       | 1        | each | 1.00  | 1.00  | 73. Green Ash           | 1        | each | 1.00  | 1.00  |
| 64. White Pine          | 1        | each | 1.00  | 1.00  | 74. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 65. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 75. White Birch         | 1        | each | 1.00  | 1.00  |
| 66. Norway Spruce       | 1        | each | 1.00  | 1.00  | 76. Green Ash           | 1        | each | 1.00  | 1.00  |
| 67. White Pine          | 1        | each | 1.00  | 1.00  | 77. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 68. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 78. White Birch         | 1        | each | 1.00  | 1.00  |
| 69. Norway Spruce       | 1        | each | 1.00  | 1.00  | 79. Green Ash           | 1        | each | 1.00  | 1.00  |
| 70. White Pine          | 1        | each | 1.00  | 1.00  | 80. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 71. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 81. White Birch         | 1        | each | 1.00  | 1.00  |
| 72. Norway Spruce       | 1        | each | 1.00  | 1.00  | 82. Green Ash           | 1        | each | 1.00  | 1.00  |
| 73. White Pine          | 1        | each | 1.00  | 1.00  | 83. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 74. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 84. White Birch         | 1        | each | 1.00  | 1.00  |
| 75. Norway Spruce       | 1        | each | 1.00  | 1.00  | 85. Green Ash           | 1        | each | 1.00  | 1.00  |
| 76. White Pine          | 1        | each | 1.00  | 1.00  | 86. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 77. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 87. White Birch         | 1        | each | 1.00  | 1.00  |
| 78. Norway Spruce       | 1        | each | 1.00  | 1.00  | 88. Green Ash           | 1        | each | 1.00  | 1.00  |
| 79. White Pine          | 1        | each | 1.00  | 1.00  | 89. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 80. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 90. White Birch         | 1        | each | 1.00  | 1.00  |
| 81. Norway Spruce       | 1        | each | 1.00  | 1.00  | 91. Green Ash           | 1        | each | 1.00  | 1.00  |
| 82. White Pine          | 1        | each | 1.00  | 1.00  | 92. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 83. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 93. White Birch         | 1        | each | 1.00  | 1.00  |
| 84. Norway Spruce       | 1        | each | 1.00  | 1.00  | 94. Green Ash           | 1        | each | 1.00  | 1.00  |
| 85. White Pine          | 1        | each | 1.00  | 1.00  | 95. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 86. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 96. White Birch         | 1        | each | 1.00  | 1.00  |
| 87. Norway Spruce       | 1        | each | 1.00  | 1.00  | 97. Green Ash           | 1        | each | 1.00  | 1.00  |
| 88. White Pine          | 1        | each | 1.00  | 1.00  | 98. Red-barked Elm      | 1        | each | 1.00  | 1.00  |
| 89. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 99. White Birch         | 1        | each | 1.00  | 1.00  |
| 90. Norway Spruce       | 1        | each | 1.00  | 1.00  | 100. Green Ash          | 1        | each | 1.00  | 1.00  |
| 91. White Pine          | 1        | each | 1.00  | 1.00  | 101. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 92. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 102. White Birch        | 1        | each | 1.00  | 1.00  |
| 93. Norway Spruce       | 1        | each | 1.00  | 1.00  | 103. Green Ash          | 1        | each | 1.00  | 1.00  |
| 94. White Pine          | 1        | each | 1.00  | 1.00  | 104. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 95. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 105. White Birch        | 1        | each | 1.00  | 1.00  |
| 96. Norway Spruce       | 1        | each | 1.00  | 1.00  | 106. Green Ash          | 1        | each | 1.00  | 1.00  |
| 97. White Pine          | 1        | each | 1.00  | 1.00  | 107. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 98. Eastern White Pine  | 1        | each | 1.00  | 1.00  | 108. White Birch        | 1        | each | 1.00  | 1.00  |
| 99. Norway Spruce       | 1        | each | 1.00  | 1.00  | 109. Green Ash          | 1        | each | 1.00  | 1.00  |
| 100. White Pine         | 1        | each | 1.00  | 1.00  | 110. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 101. Eastern White Pine | 1        | each | 1.00  | 1.00  | 111. White Birch        | 1        | each | 1.00  | 1.00  |
| 102. Norway Spruce      | 1        | each | 1.00  | 1.00  | 112. Green Ash          | 1        | each | 1.00  | 1.00  |
| 103. White Pine         | 1        | each | 1.00  | 1.00  | 113. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 104. Eastern White Pine | 1        | each | 1.00  | 1.00  | 114. White Birch        | 1        | each | 1.00  | 1.00  |
| 105. Norway Spruce      | 1        | each | 1.00  | 1.00  | 115. Green Ash          | 1        | each | 1.00  | 1.00  |
| 106. White Pine         | 1        | each | 1.00  | 1.00  | 116. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 107. Eastern White Pine | 1        | each | 1.00  | 1.00  | 117. White Birch        | 1        | each | 1.00  | 1.00  |
| 108. Norway Spruce      | 1        | each | 1.00  | 1.00  | 118. Green Ash          | 1        | each | 1.00  | 1.00  |
| 109. White Pine         | 1        | each | 1.00  | 1.00  | 119. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 110. Eastern White Pine | 1        | each | 1.00  | 1.00  | 120. White Birch        | 1        | each | 1.00  | 1.00  |
| 111. Norway Spruce      | 1        | each | 1.00  | 1.00  | 121. Green Ash          | 1        | each | 1.00  | 1.00  |
| 112. White Pine         | 1        | each | 1.00  | 1.00  | 122. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 113. Eastern White Pine | 1        | each | 1.00  | 1.00  | 123. White Birch        | 1        | each | 1.00  | 1.00  |
| 114. Norway Spruce      | 1        | each | 1.00  | 1.00  | 124. Green Ash          | 1        | each | 1.00  | 1.00  |
| 115. White Pine         | 1        | each | 1.00  | 1.00  | 125. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 116. Eastern White Pine | 1        | each | 1.00  | 1.00  | 126. White Birch        | 1        | each | 1.00  | 1.00  |
| 117. Norway Spruce      | 1        | each | 1.00  | 1.00  | 127. Green Ash          | 1        | each | 1.00  | 1.00  |
| 118. White Pine         | 1        | each | 1.00  | 1.00  | 128. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 119. Eastern White Pine | 1        | each | 1.00  | 1.00  | 129. White Birch        | 1        | each | 1.00  | 1.00  |
| 120. Norway Spruce      | 1        | each | 1.00  | 1.00  | 130. Green Ash          | 1        | each | 1.00  | 1.00  |
| 121. White Pine         | 1        | each | 1.00  | 1.00  | 131. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 122. Eastern White Pine | 1        | each | 1.00  | 1.00  | 132. White Birch        | 1        | each | 1.00  | 1.00  |
| 123. Norway Spruce      | 1        | each | 1.00  | 1.00  | 133. Green Ash          | 1        | each | 1.00  | 1.00  |
| 124. White Pine         | 1        | each | 1.00  | 1.00  | 134. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 125. Eastern White Pine | 1        | each | 1.00  | 1.00  | 135. White Birch        | 1        | each | 1.00  | 1.00  |
| 126. Norway Spruce      | 1        | each | 1.00  | 1.00  | 136. Green Ash          | 1        | each | 1.00  | 1.00  |
| 127. White Pine         | 1        | each | 1.00  | 1.00  | 137. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 128. Eastern White Pine | 1        | each | 1.00  | 1.00  | 138. White Birch        | 1        | each | 1.00  | 1.00  |
| 129. Norway Spruce      | 1        | each | 1.00  | 1.00  | 139. Green Ash          | 1        | each | 1.00  | 1.00  |
| 130. White Pine         | 1        | each | 1.00  | 1.00  | 140. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 131. Eastern White Pine | 1        | each | 1.00  | 1.00  | 141. White Birch        | 1        | each | 1.00  | 1.00  |
| 132. Norway Spruce      | 1        | each | 1.00  | 1.00  | 142. Green Ash          | 1        | each | 1.00  | 1.00  |
| 133. White Pine         | 1        | each | 1.00  | 1.00  | 143. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 134. Eastern White Pine | 1        | each | 1.00  | 1.00  | 144. White Birch        | 1        | each | 1.00  | 1.00  |
| 135. Norway Spruce      | 1        | each | 1.00  | 1.00  | 145. Green Ash          | 1        | each | 1.00  | 1.00  |
| 136. White Pine         | 1        | each | 1.00  | 1.00  | 146. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 137. Eastern White Pine | 1        | each | 1.00  | 1.00  | 147. White Birch        | 1        | each | 1.00  | 1.00  |
| 138. Norway Spruce      | 1        | each | 1.00  | 1.00  | 148. Green Ash          | 1        | each | 1.00  | 1.00  |
| 139. White Pine         | 1        | each | 1.00  | 1.00  | 149. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 140. Eastern White Pine | 1        | each | 1.00  | 1.00  | 150. White Birch        | 1        | each | 1.00  | 1.00  |
| 141. Norway Spruce      | 1        | each | 1.00  | 1.00  | 151. Green Ash          | 1        | each | 1.00  | 1.00  |
| 142. White Pine         | 1        | each | 1.00  | 1.00  | 152. Red-barked Elm     | 1        | each | 1.00  | 1.00  |
| 143. Eastern White Pine | 1        | each | 1.00  | 1.00  | 153. White Birch        | 1        | each | 1.00  | 1.00  |
| 144. Norway Spruce      | 1        | each |       |       |                         |          |      |       |       |



# Development Review Committee

## Approved Minutes

Development Application, #PL201600147

Mtg Date: September 13, 2016

McLeod Conference Room

Bloomington Civic Plaza

1800 West Old Shakopee Road

### Staff Present:

Kris Kaiser (Fire Prev) 952-563-8968

Duke Johnson (Bldg & Insp) 952-563-8959

Bruce Bunker (Eng.) 952-563-4546

Eric Wharton (Utilities) 952-563-4579

Erik Solie (Env. Health) 952-563-8978

Don Palmer (Assessing) 952-563-4644

Nick Johnson (Planning) 952-563-8925

Vicky Soukaseum (Eng.) 952-563-4627

### Project Information:

|                     |  |
|---------------------|--|
| Project             | Peace Lutheran Church Parking Lot  |
| Site Address        | 8600 E BUSH LAKE RD, BLOOMINGTON, MN 55438   |
| Plat Name           | PEACE LUTHERAN CHURCH ADDITION;  |
| Project Description | Conditional Use Permit amendment for an addition to a place of assembly, Final Site and Building Plans for a building addition and parking lot improvement, and a Variance to reduce the required side yard setback from 60 feet to 30 feet for a building addition. |
| Application Type    | Conditional Use Permit<br>Final Site and Building Plan<br>Variance   |
| Staff Contact       | Nick Johnson   |
| Applicant Contact   | PEACE EVANGELICAL LUTH CH <NO PRIMARY PHONE>   |
| PC                  | 09/29/2016   |
| CC (tentative)      | 10/24/2016   |

### Guests Present:

| Name          | Email  |
|---------------|--|
| Lori Thomson  | <a href="mailto:Lori_thomson@msn.com">Lori_thomson@msn.com</a>   |
| Paul Anderson | <a href="mailto:panderson@alliance.us">panderson@alliance.us</a> |

### Discussion/Comments:

- Nick Johnson (Planning):
  - Proposing a building addition to their church which include kitchen facilities, offices, and restroom facilities. Also, a 30 foot setback variance to East Bush Lake Road to accommodate the expansion. In addition to the building improvement, proposed are parking lot improvements and the demolition of a duplex in order to construct a surface parking lot located northwest of the church that will be connected via pedestrian sidewalk.
  - The application involves a variance, amendment for Conditional Use Permit for place of assembly in R-1 zoning district, and Final Site and Building Plans.
  - Part of the Conditional Use Permit, it is non-concurring use agreement, to bring the entire parking in concurrence of the City code. Use a certain amount space during a certain time.

- Don Palmer (Assessing):
  - No Comment.
- Erik Solie (Environmental Health):
  - No Comment.
- Duke Johnson (Building and Inspection):
  - Bathroom stalls are not up to Minnesota State Handicap Code requirement- not sufficient space.
  - D. Johnson asked is the nursery going to have smaller restrooms or unisex?
    - Anderson answered, they are unisex bathrooms.
    - D. Johnson responded, that's fine, please have compliance with MN State Handicap code requirements.
- Kris Kaiser (Fire Prevention):
  - Combination domestic sprinkler system into the building. Kaiser asked, if the existing building is fully sprinklered?
    - Both Thomson and Anderson replied no, but it is planning to be.
  - Hydrant to be located within 50 feet of Fire Department Connection.
  - Concerns about the AutoTurn with 20 feet turn radius. Need AutoTurn to ensure a large enough turn radius.
    - N. Johnson replied it has been grandfathered in as legally non-conforming.
- Bruce Bunker (Engineering) provided the Public Works comments and noted the following:
  - Proposed retaining wall needs to vacate drainage and utility easements. Encroachment agreement application must be submitted for encroachment in public right-of-way or easements.
    - Bunker provided encroachment agreement application to Anderson and Thomson.
  - Need stormwater management plan meeting requirements of Bloomington Comprehensive Surface Water Management Plan.
  - Stormwater maintenance agreement to be recorded and documented with the County.
  - Submit the Nine Mile Creek Watershed District permit prior to issuance of any city permits.
- Eric Wharton (Utilities):
  - Need SAC (Sewer Availability Charge) determination because the change of use and space.
  - Taps of live watermain are done by City forces and paid for and coordinated with Contractor.
  - Hydrants aren't meeting the 150 feet radius of the building requirement.
  - Alternative loop is to extend the watermain that is on E Bush Lake Road south, and tie into the hydrant. Contact your Engineer for more detail and options.

- Nick Johnson (Planning):
  - City code prohibits coating of exterior walls in R-1 zoning district. There have been instances where City has granted variances for this, encouraged to contact Planning staff before doing so.
    - Anderson replied, it is under his understanding that penetrating stain or fiber cement board are approved options that they are considering.
    - N. Johnson confirmed.
  - Landscape plan is missing some trees in the parking island.
  - Screening must comply with City code, 5 foot yearlong screening. Options are opaque wall or fence, a berm, or two rows of evergreen trees. Given it is a five foot landscape yard, berm or evergreens might not fit, so wall or fence might be more likely.
  - Need approved lighting plan before building permit is issued.
  - Relocation of playground needs a 15 foot setback to side and rear property lines and Minor Revision to Final Site and Building Plans. Administrative zoning application.
- Paul Anderson:
  - There are additional parking stalls if they get approval for non-concurrent use, they may be able to get two rows of trees for screening.
    - N. Johnson responded, by shifting all the parking stalls, there may be loss of drive aisles.



## Comment Summary

PL201600130

**Application #:** PL201600130

**Address:** 7103 W 86TH ST, BLOOMINGTON, MN 554387101 W 86TH ST, BLOOMINGTON, MN 554388600 E BUSH LAKE RD, BLOOMINGTON, MN 55438.

**Request:** **CUP Amendment and Final Site and Building Plans for a 2,774 square foot addition to an existing place of assembly and parking lot improvements**

**Meeting:** Pre-Application DRC - August 02, 2016  
Post Application DRC -  
Planning Commission -  
City Council -

---

**Planning Review - Pre-App Contact:** Nick Johnson at nmjohnson@BloomingtonMN.gov, (952) 563-8925

- 1) Total use of the facility would require approximately 175 parking spaces. 115 parking stalls are proposed to be constructed, with 21 proof of parking stalls proposed, resulting in a total of 136 stalls. The applicant must work with Planning Staff to clarify and sharpen required amount of parking and concurrent use scenarios.
- 2) Parking stalls must be 9 feet in width.
- 3) All parking islands must be 8 feet in internal width.
- 4) All parking islands must have 1 deciduous tree.
- 5) A Landscaping Plan is required. The applicant must demonstrate compliance with Section 19.52 of the City Code. 143 total shrubs are required on site.
- 6) A three foot high landscaping screen for the parking lot adjacent to the public street is required.
- 7) 5-foot year-round screen required per 19.52.
- 8) Primary exterior building materials must not be coated per Section 19.63.08(b) of the City Code.
- 9) Parking lot and exterior security lighting must meet Section 21.301.07. A minimum of 1.0 foot-candles is required on the parking surface (which may be reduced to 0.5 foot-candles for the outer perimeter of the parking lot - 25 feet).
- 10) Private sidewalks must be a minimum width of 5 feet.

**Fire Department Review - Pre-App Contact:** Laura McCarthy at lmccarthy@BloomingtonMN.gov, (952) 563-8965

- 1) Minimum 20' wide emergency vehicle access lane with turning radius to accommodate BFD ladder 1 throughout the parking areas. Access road shall support the heaviest emergency vehicle - 40 tons.
- 2) Entire building is required to be sprinklered with a fire alarm system for an assembly and group E occupancies.
- 3) Maintain hydrant coverage within 150 feet of all parts of the building.

**Public Works Review - Pre-App Contact:** Jen Desrude at [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov), (952) 563-4862

- 1) Private common utility easement/agreement must be provided.
- 2) A signed copy of private common parking/access easement agreement shall be provided.
- 3) Utility as-builts must be provided prior to issuance of Certificate of Occupancy.
- 4) A Tier 2 Transportation Demand Management (TDM) checklist is required.
- 5) Contractor shall obtain a Public Works permit for obstructions and concrete work within the right-of-way. Permit is required prior to removals or installation. Contact Brian Hansen (952-563-4543, [bhansen@BloomingtonMN.gov](mailto:bhansen@BloomingtonMN.gov)) for permit information.
- 6) Contractor shall obtain a Public Works permit for underground work within the right-of-way. Permit is required prior to removals or installation. Contact Utilities (952-563-4568) for permit information.
- 7) Existing drainage and utility easements may be vacated. Contact Bruce Bunker at 952-563-4546 or [bbunker@BloomingtonMN.gov](mailto:bbunker@BloomingtonMN.gov) for a copy of the Public Rights-of-Way Vacation Application. Application fee = \$300 per application, no matter the number of easements. It is the responsibility of the developer to determine if private utilities exist in the easement prior to submitting the application. Developer/owner to provide legal description and Engineering staff will prepare vacation document.
- 8) Temporary street signs, lighting, and addresses shall be provided during construction.
- 9) Restore City street by complying with the City Street Improvement Policy; contact Utilities (952-563-4568) for the requirements and show this on the plan.
- 10) Connection charges shall be due prior to the issuance of utility permits - dollar amount to be determined.

**Traffic Review - Pre-App Contact:** Jen Desrude at [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov), (952) 563-4862

- 1) Dimension the drive aisles on the new upper parking.
- 2) The keyed note calls it heavy duty pavement but the reference detail calls it light duty. Please clarify.
- 3) All parking stall striping must be painted white. Parking islands must be 3-feet shorter than the parking stall and 8-feet wide.

- 4) A MN licensed Civil Engineer must design and sign all retaining walls 4-foot high and higher.
- 5) All construction and post-construction parking and storage of equipment and materials must be on-site. Use of public streets for private construction parking, loading/unloading, and storage will not be allowed.
- 6) Show and label all property lines and easements on all plan sheets.
- 7) Move landscaping, pond, retaining wall, or other structure out of right-of-way or easement.
- 8) Bicycle parking spaces shall be provided (i.e., bike rack), number to be approved by the City Engineer. Show location onsite plan and rack, detail in C4.0 and C5.0
- 9) All public sidewalks shall not be obstructed.
- 10) Storage of materials or equipment shall not be allowed on public streets or within public right-of-way. (Add to removal, utility or site plan sheets)
- 11) A Bloomington standard non-residential driveway apron is required on accesses to public streets (see detail).
- 12) Private underground facilities owned by the customer are those private facilities installed typically behind the meter, like irrigation lines, invisible dog fences, parking lot lighting and others. The property owner or tenant is responsible to locate those private facilities or hire someone to locate these lines during construction when a Gopher State One Call ticket is placed.
- 13) Use B618 curb and gutter in the right-of-way.
- 14) Discrepancy between note and plan dimensions

**Utility Review - Pre-App Contact:** Jen Desrude at [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov), (952) 563-4862

- 1) Use updated city standard details for driveways, utilities, erosion control, etc. found on the website at [www.bloomingtonmn.gov/information-sheets-and-handouts-engineering-division](http://www.bloomingtonmn.gov/information-sheets-and-handouts-engineering-division)
- 2) Provide peak hour and average day water demand and wastewater flow estimates
- 3) Water meters should be located immediately after the water service enters the interior of the new addition.
- 4) Sewer Availability Charge (SAC) be satisfied. Contact the Met Council at 651-602-1378 for a SAC determination, which is required by the Metropolitan Council Environmental Services.
- 5) A minimum 10-foot horizontal separation and 18-inch vertical separation is required between watermain and sewers.
- 6) Loop water system (supply from two points) to provide increased reliability of service and reduce head loss. This depends on water supply needs. Provide fire flow testing.
- 7) Provide valves for system isolation (longest interval cannot exceed 400 feet) and for building isolation without shutting down supply to hydrants.
- 8) Install enough hydrants to provide fire protection for the entire building (each hydrant covers a 150' radius). New hydrant location exceeds 240'.

- 9) Provide a minimum of 8-feet and a maximum of 10-feet of cover over all water lines, valves, services, etc.
- 10) Use Class 52 DIP water main for pipe 12-inches in diameter and smaller. A minimum 8 mil polywrap is required on all DIP.
- 11) Approval of plans and specifications for this development are to be approved by the Minnesota Department of Health (MDH). It is the responsibility of the developer to:
  - o Submit the required signed documents and fees to the MDH including the MDH Plan Review Fee Sheet.
  - o Provide a copy of the MDH approval letter for the project to the City of Bloomington.Information regarding the MDH Plan Review may be obtained by visiting the MDH Environmental Health Division website:  
[www.health.state.mn.us/divs/eh/water/planreview/index.html](http://www.health.state.mn.us/divs/eh/water/planreview/index.html). No permits will be issued before the City has received the MDH Plan Approval Letter. Please note that MDH review may take up to 6 weeks.
- 12) Taps of live water mains are done by City forces and paid for and coordinated with the Contractor.
- 13) Utility and mechanical contractors shall coordinate installation of water service pipes, fittings, and valves all the way into the building (i.e. up to meters and/or fire service equipment) to accommodate City inspection and testing. Utility and mechanical contractors shall coordinate installation of sewer services all the way into the building to accommodate City inspection and testing.
- 14) Combination fire and domestic services must terminate with a thread on flange or an MJ to flange adapter.
- 15) All components of the water system, up to the water meter or fire service equipment must utilize protective internal coatings meeting current ANSI/AWWA standards for cement mortar lining or special coatings. The use of unlined or uncoated pipe is not allowed.

**Water Resources Review - Pre-App Contact:** Jen Desrude at [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov), (952) 563-4862

- 1) Provide soil boring data.
- 2) A Stormwater Management Plan/Report shall be provided which includes:
  - o Stormwater Rate Control – No net increase in runoff.
  - o Storm Water Volume Control – no increase in volume.
  - o Water Quality Treatment meeting requirements of Bloomington Comprehensive Surface Water Management Plan (CSWMP)  
<http://bloomingtonmn.gov/cityhall/dept/pubworks/engineer/waterres/mgmtplan/surfacewtr/surfacewtr.htm>
  - o Maintenance Schedule/Plan for Stormwater BMP signed by property owner to be filed on record with Hennepin County. Proof of filing must be submitted to Engineering.After City staff approves the Stormwater Management plans, an extra set of plans and Stormwater Management plan will be needed for submittal to Lower Minnesota Watershed



District for review and comment.

Provide modeling/Hydro CAD calculations.

3) An NPDES construction site permit and SWPPP shall be provided. The name and phone number of party responsible for erosion control shall be included; if greater than, one acre is disturbed.

Review SWPPP, project narrative incomplete, quantities.

4) Erosion Control Bond required prior to issuance of permits – dollar amount based on the table below. Contractor to provide bid prices to install, maintain and remove EC devices.

Disturbed Area (acres)

Surety amount

0.00-0.50

\$5,000

0.51-0.75

\$8,000

0.76-1.00

\$11,000

Greater than one acre

\$0.25 per square foot of disturbed area rounded  
to the nearest \$1,000 (maximum \$25,000)

5) An Erosion Control Plan shall be provided which includes:

- o Erosion Control BMP locations shown on the plan.
  - o Notes for maintenance (1/3 capacity, damage, tracking onto streets) and inspection (who is responsible, frequency), etc., consistent with the MPCA Protecting water Quality in Urban Areas BMP Manual (Nov. 2000).
  - o Use of updated City of Bloomington Standard Details from the City of Bloomington website: <http://bloomingtonmn.gov/information-sheets-and-handouts-engineering-division>
  - o No bales allowed for inlet protection and/or ditch checks.
  - o All materials shall meet MnDOT approved materials list: [www.mrr.dot.state.mn.us/materials/apprprod.asp](http://www.mrr.dot.state.mn.us/materials/apprprod.asp)
  - o Use approved inlet protection at all active storm sewer inlets; only basket or sack style in traffic areas.
  - o Include turf establishment plan.
- See notes on plan.

6) Utility permits are required for connections to the public storm, sanitary, and water system. Contact Utilities (952-563-8777) for permit information.

- 7) Submit a copy of Nine Mile Creek Watershed District permit and comments prior to issuance of City of Bloomington permits ([www.ninemilecreek.org](http://www.ninemilecreek.org))
- 8) Silt fence needed.
- 9) Inlet protection needed
- 10) Spelling,  
Infiltration
- 11)
- 12) Nine Mile Creek Watershed District

**Environmental Health Review - Pre-App Contact:** Erik Solie at  
[esolie@BloomingtonMN.gov](mailto:esolie@BloomingtonMN.gov), (952) 563-8978

- 1) If they are using the kitchen for any licensed activity, such as school, preschool or daycare not related to church activity, they must comply with current food code requirements.

Originator  
Community Development

Item  
**Type II Prelim & Final Plat, Maj Rev FDP**

Date  
9/29/2016

Description

## GENERAL INFORMATION

**Applicant:** Bloomington Central Station LLC c/o McGough Development

**Location:** 8170 31<sup>st</sup> Avenue South

**Request:** Preliminary and Final Plat for Bloomington Central Station 5th Addition; Major Revision to Final Development Plan for a 1,666 stall parking structure and surface parking improvements.

## AGENCY ACTION DEADLINE AND PUBLIC NOTIFICATION

Application Date: 08/24/16  
60 Days: 10/23/16  
120 Days: 12/22/16  
**Applicable Deadline: 10/23/16**  
Newspaper Notification: Confirmed – (09/15/16 Sun Current – 10 day notice required)  
Direct Mail Notification: Confirmed – (500 foot buffer – 10 day notice required)

## STAFF CONTACT

Nick M. Johnson, Planner  
(952) 563-8925 – [nmjohnson@BloomingtonMN.gov](mailto:nmjohnson@BloomingtonMN.gov)

---

## Requested Action

Staff recommends approval of the Preliminary and Final Plat and Major Revision to Final Development Plans through the following motions:

In Case PL2016-146, having been able to make the required findings, I move to recommend approval of a Preliminary and Final Plat for Bloomington Central Station 5th Addition, subject to the conditions and Code requirements attached to the staff report.

In Case PL2016-146, having been able to make the required findings, I move to recommend approval of a Major Revision to Final Development Plans to construct a 1,666 stall parking structure and surface parking improvements at 8170 31st Avenue South, subject to the conditions and Code requirements attached to the staff report.

---

Attachments:

Staff Report  
Notification Map  
Affidavit of Publication  
Project Description  
Plans  
DRC Minutes  
DRC Comment Summary

## GENERAL INFORMATION

|                                  |   |
|----------------------------------|---|
| Applicant:                       | Bloomington Central Station LLC c/o McGough Development   |
| Location:                        | 8170 31st Avenue South  |
| Request:                         | Preliminary and Final Plat for Bloomington Central Station 5 <sup>th</sup> Addition; Major Revision to Final Development Plan for a 1,666 stall parking structure and surface parking improvements.   |
| Existing Land Use and Zoning:    | Vacant commercial land; zoned High Intensity Mixed Use with Residential (Planned Development) – HX-R (PD) and Airport Runway – AR-17  |
| Surrounding Land Use and Zoning: | North – Park, surface parking and light rail transit right-of-way; zoned HX-R (PD)<br>South – Office and vacant land; zoned C-4<br>East – Multiple-family dwelling; zoned HX-R (PD)<br>West – Structure parking ramp, office and manufacturing; zoned LX and CO-1 |
| Comprehensive Plan Designation:  | South Loop Mixed Use  |

## HISTORY

|                      |   |
|----------------------|---|
| City Council Action: | 10/06/08 – Approved Preliminary and Final Plat for Bloomington Central Station 2 <sup>nd</sup> Addition (Case 2830A-08) |
|                      | 07/24/06 – Approved revised Preliminary and Final Development Plans for a temporary parking lot (Case 2830CD-06)        |
|                      | 2/06/06 Approved Revised Preliminary Development Plan for transit oriented mixed use development (Case 2830C-05)        |

## CHRONOLOGY

|                     |            |                          |
|---------------------|------------|--------------------------|
| Planning Commission | 09/29/2016 | Public Hearing Scheduled |
|---------------------|------------|--------------------------|

## PROPOSAL

McGough Development is proposing to construct an eight-story (1 story below grade, 7 stories above grade) parking ramp structure with 1,666 parking stalls to serve the Health Partners office tower and future development within Bloomington Central Station (BCS). The parking ramp will have the ability to be expanded southward to a total of 2,700 parking stalls through a future phase of construction. In order to construct the proposed parking structure, the developer would plat a portion of Bloomington Central Station 2<sup>nd</sup> Addition into one buildable lot and three outlots. Additional physical site improvements proposed include the construction of 82<sup>nd</sup> Street and a portion of 31<sup>st</sup> Avenue South (both private roadways), the reconfiguration of the existing Health Partners office tower loading dock, and the improvement of existing surface parking lots west of the Health Partners building. McGough Development also requests an administratively issued Airport Zoning Permit for the construction of the parking structure, as the ramp is greater than 80 feet in height.

## ANALYSIS

### Land Use

The original Preliminary Development Plan (PDP) for Bloomington Central Station was approved in 2006. As guided by the approved Preliminary Development Plan, the subject area anticipated an office building (11 stories, 356,000 square feet) and parking ramp (3,060 parking spaces). At this time, the developer is requesting to proceed on the parking ramp to meet contractual parking obligations with Health Partners and prepare portions of Bloomington Central Station for future development. The planned office building proposed is currently on-hold. While the approved Preliminary Development Plan included a 3,060 stall parking structure, the proposed plan would reduce the total number of parking stalls in the structure to 2,700. The eight-story parking structure would be constructed in two phases, with Phase 1 housing 1,666 stalls. Although the total number of parking stalls would be reduced by 360 spaces, the proposed land use is consistent with the approved PDP due to the fact that no office development is proposed with this application. The developer understands that the reduction in parking may reduce the overall square footage of future office development on the subject property. To summarize, the proposed parking ramp would not increase the intensity of overall development as regulated by the PDP. As a result, a revision to the PDP is not required as part of this application given the proposed scope of work.

### Code Compliance

The proposed parking ramp, surface parking improvements and other elements of the proposed scope of work have been reviewed according to the performance standards for structures in the HX-R zoning district and other specific sections of the City Code. Table 1 provides an overall analysis of Code compliance for the subject development. The sections that follow provide follow-up information with greater detail to inform recommended conditions of approval and modifications to the proposed plans.

**Table 1: City Code Analysis for Parking Structures in HX-R Zoning District**

| Standard and Code Section  | Proposed   | Meets Standard?                   |
|--|--|-----------------------------------|
| <b>Building Setbacks</b><br>Public street setback – 0 feet<br>Internal setbacks – 10 feet<br>(Section 19.29(h)(1))   | Setbacks (smallest proposed):<br>Public street setback – 1.7 feet<br>Internal setbacks – 10 feet                                 | Yes                               |
| <b>Minimum Lot Size</b> – 120,000 square feet or part of a PD 120,000 square feet or larger (Section 19.29(h)(2))  | Bloomington Central Station PD<br>– 1,744,422 square feet  | Yes                               |
| <b>Surface Parking Stall Dimensions</b> – 9 feet by 18 feet for 90 degree parking (Section 21.301.06(c)(2)(C)(i))  | All surface parking stalls on site are 9’x18’  | Yes                               |
| <b>Structure Parking Stall Dimensions</b> – 8.5 feet by 18 feet for 90 degree parking (Section 21.301.06(h)(2)(C))   | Adequate dimensions provided   | Yes                               |
| <b>Drive Aisles</b> - Aisles adjacent to 90 degree parking must be 24 feet wide. All other drive aisles must be a minimum of 20 feet per Fire Code (Section 21.301.06(c)(2)(C)) and Section 21.301.06(h)(2)(C)). | All drive aisles within surface and structure parking areas are 24 feet in width.  | Yes                               |
| <b>Off-Street Loading</b> – Space must be provided with loading area so that all maneuvering is conducted outside of the public right-of-way (Section 21.301.06(j)(1)).  | The width of the proposed loading dock is 75 feet. The adjacent roadway is private right-of-way.                                 | Yes                               |
| <b>Sidewalk Width</b> - Private sidewalks must have a minimum width of five unobstructed feet. Sidewalks perpendicular to 90 degree parking must be seven feet in width (Section 21.301.04(d)(1)(B)).            | All sidewalks are at least 5 feet in width. One sidewalk (south lot) perpendicular to 90 degree parking must be 7 feet in width. | Revisions required (see comments) |
| <b>Landscaping – Trees / Shrubs</b> – One tree per 2,500 sq. ft. and one shrub per 1,000 sq. ft. of disturbed area (Sec 19.52(c)(2)). 146 trees and 366 shrubs required.   | 143 trees<br>383 shrubs (not including perennials)   | Revisions required (see comments) |

| Standard and Code Section  | Proposed  | Meets Standard?                           |
|--|---|---|
| <b>Landscape Yard</b> –20 feet adjacent to streets and 5 feet for interior lot lines (Section 19.52(c)(4)).                              | Landscape yard to 30 <sup>th</sup> Ave S:<br>Existing – 12.3 feet<br>Proposed – 17.3 feet                       | No - deviation recommended (see comments) |
| <b>Parking Island Trees</b> – A minimum of one deciduous tree must be provided per parking lot island (Section 21.301.06(c)(2)(H)(iii)). | Trees are provided in all new and modified islands except for three islands along East 82 <sup>nd</sup> Street. | Revisions required (see comments)         |
| <b>Screening</b> – Perimeter parking lot screening required for surface lots (Section 19.52(d)(4)(A)).                                   | No screening provided for west parking lot along 30 <sup>th</sup> Ave. S.                                       | Revisions required (see comments)         |
| <b>Lighting for Parking Surface</b> - 1.5 foot-candle required (Section 21.301.07(c)(13))  | Maintained light levels for surface parking do not meet Code in multiple areas.                                 | Revisions required (see comments)         |

### **Platting**

In order to construct the proposed parking structure, the subject area must be platted (the site is currently within an outlot). The applicant has submitted a Preliminary and Final Plat application in conjunction with the Final Development Plan (FDP). The plat, Bloomington Central Station 5<sup>th</sup> Addition, includes one buildable lot (90,600 square feet) for the parking structure, and three outlots (A, B and C). The buildable lot (Lot 1, Block 1) is located in the northwest corner of the subject area and is bounded by 30<sup>th</sup> Avenue South on the west and the Blue Line Light Rail Transit (LRT) right-of-way on the north. Outlot A, directly to the south and east of the buildable lot, is 120,885 square feet in size and is proposed to remain as an improved surface parking lot until an office building is constructed there. Outlot B is 29,916 sq. ft. in size and would be utilized as private right-of-way for East 82<sup>nd</sup> Street. Finally, Outlot C is 124,307 square feet in size and would remain as a surface parking lot until future development occurs. Should the plat be approved, it must be recorded at Hennepin County prior to the issuance of any building permits.

### **Building Design**

The proposed parking structure would be constructed of pre-cast concrete panels with multiple finishes intended to create a pattern, breaking up the mass of the façade. The pre-cast concrete panels would be varying tones of gray in color. The color palette is intended to match the architectural theme and design of other structure in Bloomington Central Station. The north and south elevations of the ramp include precast white columns that are intended to break up the mass of the parking structure. The High Intensity Mixed Use with Residential (HX-R) zoning district contains performance standards for the design of parking structures. A review of the design of the proposed parking structure in the context of said performance standards is provided in Table 2. In



staff's judgment, the proposed parking structure meets the intent of the architectural performance standards and is compliant with City Code in terms of design and building materials.

**Table 2: Architectural Performance Standards for Parking Structures in HX-R Zoning District**

| Standard and Code Section   | Proposed   | Meets Standard? |
|---|--|-----------------|
| <b>Exterior Materials Standards –</b><br>Parking structure exterior materials must meet the requirements of Section 19.63.08 of the City Code (Section 19.29(j)(6)(B)(i)).                      | Precast architectural concrete and glass   | Yes             |
| <b>Complimentary Materials –</b><br>Parking structure exterior material color must complement the color of the building or buildings the parking structure serves (Section 19.29(j)(6)(B)(ii)). | Health Partners office building is predominantly glass. Light and medium gray precast concrete with glass elements on the ramp compliment the office building.                               | Yes             |
| <b>Architectural Elements -</b> Parking structures must include architectural elements that enhance the structure, break up its mass (Section 19.29(j)(6)(B)(iii)).                             | Alternating light and medium gray concrete panels create a pattern to break up building mass. White pre-cast concrete column pattern on north and south elevation provide decorative effect. | Yes             |
| <b>Access Points -</b> Parking structure access points must be architecturally articulated to add visual interest (Section 19.29(j)(6)(B)(iv)).   | All vehicle access points have concrete canopies to help identify ingress/egress locations.  | Yes             |
| <b>Screening of Headlights –</b><br>Parking structures must be designed to block the visibility of vehicle headlights from outside the parking structure (Section 19.29(j)(6)(B)(v)).           | Ramp wall openings are 3.8 feet above ramp floor elevations, effectively blocking light trespass from vehicle headlights   | Yes             |

**Landscaping, Screening and Lighting**

City Code requires one tree to be planted for every 2,500 square feet and one shrub for every 1,000 square feet of developable area. The subject property requires 146 trees and 366 shrubs to be installed based upon 365,708 square feet of developable area. The applicant is proposing to install 143 trees and 383 shrubs (not counting perennial grasses). Five of the trees proposed are currently located in the public right-of-way for 30<sup>th</sup> Avenue South. According to Section 19.52(c)(5) of the City Code, plantings within the public right-of-way in areas with an approved Streetscape Plan do not receive credit for overall site landscape requirements. As a result, five of the trees proposed do not count towards site landscaping. The applicant must revise their landscape plan to include 8 additional trees on-site in order for the Landscape Plan to be Code-compliant. In addition, three proposed parking islands within the West parking area do not currently have deciduous trees. Revision to the landscape plan should include trees for all parking islands.

City Code (Sec. 19.52(c)(4)) requires a 20 foot planted area adjacent to all public rights-of-way. 30<sup>th</sup> Avenue South previously was a private right-of-way until the City recently took ownership. The site currently has an existing landscape yard of 12.3 feet. The proposed design would provide a 17.3 foot landscape yard. While the proposed landscape yard does not meet the Code-required 20 foot dimension, the landscape yard is being increased. In addition, the landscape yard requirement previously did not apply to this property boundary due to the right-of-way previously being privately owned and managed. Given these factors, staff is recommending that a deviation be provided to allow for the 17.3 foot landscape yard. All other property boundaries of the development either have Code-complying landscape yards, or are not subject to the landscape yard requirement as a result of the development being part of one larger Planned Development (PD).

In relation to screening, City Code (Sec. 19.52(d)(4)) requires the perimeter of surface parking lots adjacent to public streets have a screen that is three to four feet in height. The western section of the west parking lot requires a perimeter screen be provided along 30<sup>th</sup> Avenue South. While trees are planned along the parking lot perimeter, Code requires the screen be comprised of continuous shrubs, perennial planting, a berm or an architecturally compatible wall or fence. The area that must be screened is south of the southwest corner of the proposed parking structure, extending a distance of approximately 100 feet to 82<sup>nd</sup> Street. The landscape plan must be revised to provide Code-compliant perimeter screening. The surface parking lot south of 82<sup>nd</sup> Street, the southwest lot, is not subject perimeter screening at this time, as the area is permitted to remain as legally non-conforming temporary parking awaiting future development. It should be noted that the non-conformity arose from the City taking ownership of 30<sup>th</sup> Avenue South, thereby creating a non-compliant landscape yard.

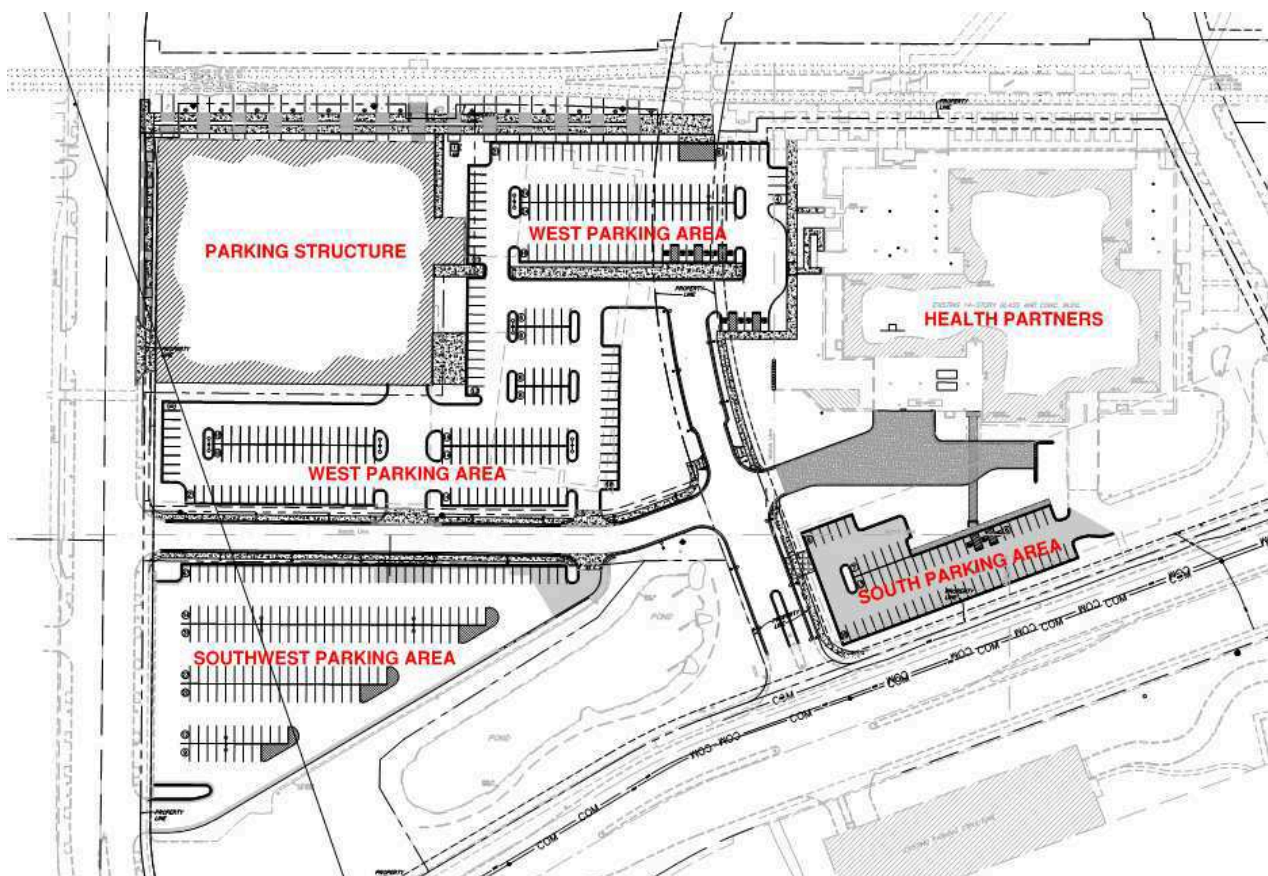
The lighting proposed for the subject area of Bloomington Central Station must meet a minimum light level of 1.5 foot-candles for surface parking, as the area is considered to be an office use. The applicant has submitted a photometric lighting plan utilizing existing and new poles with LED fixtures in all of the surface parking areas. In reviewing the plan submitted, there are multiple locations of the plan that do not meet minimum light levels. The photometric plan will need to be revised in order to meet Code. In addition, no photometric plans have been provided for lighting

within the parking structure. The interior portions of the ramp must have a minimum light level of 3.0 foot-candles. Staff is recommending a condition that a photometric lighting plans for the whole subject area, including the parking ramp, be approved prior to the issuance of building permits.

### **Parking**

Along with the parking structure, there are three areas of surface parking proposed. See Exhibit 1 for a map identifying the surface parking areas discussed in this report. Each specific surface parking area is proposed to be modified in some fashion from its existing condition.

### **Exhibit 1: Parking Areas for BCS – Health Partners**



With the scope of work proposed, the applicant would construct and/or modify a total of 2,283 parking spaces that will serve the Health Partners office building and future development within the subject area. The breakdown of the proposed parking stalls across the subject area is provided in Table 3.

**Table 3: Parking Analysis for BCS - Health Partners Area**

| Parking Areas                        | Existing Parking Spaces | Proposed Parking Spaces  |
|--------------------------------------|-------------------------|--|
| Parking Structure                    | 0                       | 1,666<br>(129 compact spaces)<br>(possible expansion to 2,700)               |
| South Parking Lot                    | 165                     | 63   |
| West Parking Lot                     | 614                     | 277  |
| Southwest Parking Lot                | 231                     | 172  |
| Health Partners<br>Executive Parking | 105                     | 105  |
| <b>Total Parking Spaces</b>          | <b>1,115</b>            | <b>2,283</b><br><b>(2,154 when not including<br/>compact parking spaces)</b> |

From a parking quantity standpoint, the proposed parking structure and site improvements would provide a net increase of 1,168 parking spaces. As noted in the applicant's project description, the parking requirement for the Health Partners office building is 1,894 parking spaces. With a total of 2,154 legal parking spaces (not including compact spaces), there is an excess of 260 parking spaces dedicated to the Health Partners office building at this time. Should the office building expansion occur according to the Bloomington Central Station PDP, construction of phase 2 of the parking structure would likely occur concurrently, adding 1,034 parking spaces. The parking calculation for future development will occur at that time. To summarize, the proposed parking improvements comply with City Code and the approved Preliminary Development Plan.

In addition to parking counts, a summary of improvements to the three surface parking areas is also warranted. Code-compliant landscaping and lighting is planned for both the South and West parking areas, whereas only Code-compliant lighting is planned for the Southwest parking area. The improvements to the Southwest parking area are considered to be more related to parking lot maintenance than permanent development. The temporary parking lots were approved to be constructed in their current condition while awaiting future development. All of the surface parking areas are intended for more permanent development at some point in the future. Nevertheless, the West and South parking areas will be improved to Code-compliant status for the interim period prior to future development.

**Access and Circulation:**

Site access will continue to be provided from East Old Shakopee Road and 30<sup>th</sup> Avenue South. The construction of East 82<sup>nd</sup> Street and a portion of 31<sup>st</sup> Avenue South, both private roadways, will allow for improved circulation throughout the surface parking areas and Health Partners site. The modified loading dock will be directly accessed from 31<sup>st</sup> Avenue South. The parking structure will have two access points.

One access is located on 30<sup>th</sup> Avenue South across from the Metro Transit Park and Ride ramp, and the other access is located at the southeast corner of the building within the West parking area. An initial proposal by the applicant had a different access layout. An access study was completed by SRF Consulting and it was recommended that the new ramp have only one access on 30<sup>th</sup> Avenue that aligns directly across from the driveway to the existing park-and-ride. The applicant agreed with the SRF recommendations and made changes to their design before submitting their application.

Pedestrian circulation will be also be improved throughout the site. Sidewalks are proposed to be added to both sides of East 82<sup>nd</sup> Street and the east side of 31<sup>st</sup> Avenue South. Staff is recommending that sidewalk be added to the west side of 31<sup>st</sup> Avenue South as well. Connection to the proposed parking structure is provided via sidewalk through the West parking area to the upper plaza of the Health Partners office building. In the future, a skyway is planned from the parking structure to the new office building and Health Partners site. Throughout all of the surface parking areas, pedestrians have a greater ability to navigate the parking lots on separated sidewalks, improving overall safety. Staff did identify one sidewalk that does not meet Code requirements for unobstructed width. A condition of approval is recommended that all sidewalks meet City Code standards. Finally, a pedestrian corridor and emergency access route is planned on the north side of the parking structure parallel to the Blue Line LRT right-of-way. The corridor will provide a landscaped greenway that will also serve as access to the north side of the Health Partners building and future development in case of an emergency. The design is similar to the existing corridor north of the Health Partners building. Finally, a pedestrian bridge connecting the South parking area to the upper plaza and main entrance and spanning over the loading dock will provide direct pedestrian access for visitors.

**Airport Zoning Permit:**

The proposed parking structure is subject to the Airport Zoning Height Limits because of the proximity of the subject property to Runway 17-35 of Minneapolis-St. Paul International Airport (MSP). The subject property is subject to Airport Safety Zones B and C, and must comply with the height limits established by the MSP Zoning Ordinance. The maximum height allowed under the Airport Height Limits at the location of the proposed parking structure ranges from approximately 912 to 955 feet above sea level. The finished floor elevation of the proposed parking structure is 816.5 feet. The greatest height of the proposed parking structure, located at the elevator tower, is 81.5 feet. As a result, the parking structure will extend as high as 898 feet above sea level at its

highest point. Thus, the proposed structure complies with the maximum building height allowed as determined by the Airport Height Limits.

In terms of permitting, the parking structure is taller than 80 feet in height, exceeding the height of structure allowed to proceed without an Airport Zoning Permit according to the MSP Zoning Ordinance. In addition, temporary crane variances are needed for the construction of the parking ramp. As a result, an Airport Zoning Permit is required in this case. The Airport Zoning Permit is reviewed and approved administratively by the City of Bloomington following a no-hazard declaration by the Federal Aviation Administration upon review of a RAA 7460-1 Airspace Study. Additionally, the temporary crane variance must be reviewed and approved by the Minneapolis-St. Paul International Airport Board of Adjustment. Prior to the issuance of any building permits, the applicant must have approval of the mobile crane variance and the Airport Zoning Permit.

**Stormwater Management:**

Stormwater will be managed to meet the City of Bloomington Comprehensive Surface Water Management Plan requirements for stormwater rate control (quantity), stormwater quality, and volume. The Stormwater Management plan calculations and narrative are under review. The plan is to utilize the existing stormwater pond located at the southwest corner of BCS, as well as two underground stormwater systems. A stormwater maintenance agreement will be required to be recorded at Hennepin County. This project is in Lower Minnesota River Valley Watershed District. The district does not have a permitting process, but plans and calculations will be sent to the district for their review.

**Utilities:**

Private sanitary sewer and watermain are proposed to be constructed to set the site up for future expansion. For example, a sanitary sewer stub will be constructed to connect under 31<sup>st</sup> Avenue for connection with a future parking ramp south of the existing Health Partners building. The

**Fire Preventions and Public Safety:**

Maintaining access, water supply, and addressing are critical factors. These factors will need to be addressed through all phases of the project. The applicant has agreed to provide landscaping techniques that won't interfere with firefighting operations including access to building openings.

All access and circulation design must meet or exceed the minimum standards for fire prevention and be maintained in accordance to the approved plan, including a surface to provide all weather driving capabilities. Apparatus access roads shall be asphalt or concrete and support a minimum of 80,000 pounds. The current turning radius within the existing site (south parking lot) at 8170 E 33<sup>rd</sup> doesn't meet the fire code.

Additional hydrants shall be strategically placed to provide an adequate water supply for firefighting operations in and around all areas of the project and subsequent development.

The structure must be addressed plainly and visible from the street or road using numbers that contrast with the background. The numbers must be a minimum of four inches, be Arabic numbers or alphabetic letters with a minimum stroke width of 0.5 inches.

Any changes made to the current plans, including building location, access roads, water supply, landscaping and addressing, shall be reviewed by the Fire Marshal to insure continued compliance with the fire code.

### **Status of Enforcement Orders**

There are no open enforcement orders for this property.

## **FINDINGS**

### **Section 21.501.03(e)(1),(2),(3),(4),(5),(6) and (7) – Final Development Plans**

#### **1. The proposed development is not in conflict with the Comprehensive Plan;**

- There is no conflict between the proposed development and the Comprehensive Plan. The proposed parking structure would serve high intensity office uses within the Bloomington Central Station development, which is guided South Loop Mixed Use. The land uses proposed are consistent with the South Loop Mixed Use land use designation.

#### **2. The proposed development is not in conflict with any adopted District Plan for the area;**

- The proposed development is consistent with the South Loop District Plan. The parking structure and supporting improvements are consistent with the development approvals for the Bloomington Central Station planned development.

#### **3. The proposed development is not in conflict with the approved Preliminary Development Plan for the site;**

- The proposed development is consistent with the adopted Preliminary Development Plan for Bloomington Central Station in terms of the proposed land use, intensity of use and location of use.

- 4. All deviations from City Code requirements are in the public interest and within the parameters allowed under the Planned Development Overlay Zoning District or have previously received variance approval;**
  - The proposed deviations relate to previously approved existing conditions on the site and would not have an adverse impact on the surrounding neighborhood. Some of the deviations stem from the roadways of Bloomington Central Station were privately owned and managed. The deviations described in the staff report are in the public interest in promoting future development of Bloomington Central Station.
- 5. The proposed development is of sufficient size, composition, and arrangement that its construction, marketing, and operation is feasible as a complete unit without dependence upon any subsequent unit;**
  - The proposed development activity will serve the parking needs of the Health Partners office building and surrounding future development. The project will allow for reduction of surface parking in the Bloomington Central Station development, serving both an existing parking demand and better preparing the site for additional development in the future. Operation of the proposed parking structure is feasible as a complete unit and not dependent on a subsequent unit.
- 6. The proposed development will not create an excessive burden on parks, schools, streets, and other public facilities and utilities which serve or are proposed to serve the planned development; and**
  - The proposed parking structure will not be adding a greater intensity of use to the Bloomington Central Station development at this time. The intent of the project is to provide more efficient parking facilities on-site in order to clear surrounding sites for future development. The proposed parking structure will not create an excessive burden on parks, schools, streets or other public facilities and utilities that serve the development.
- 7. The proposed development will not be injurious to the surrounding neighborhood or otherwise harm the public health, safety and welfare.**
  - The parking structure is consistent with the approved Preliminary Development Plan and will serve existing and future office development on surrounding parcels. The proposed development will not be injurious to the surrounding neighborhood or otherwise harm the public health, safety and welfare.

#### **Section 22.05 (d) (1-8) Preliminary Plat**

- 1. The plat is not in conflict with the Comprehensive Plan;**



- The plat is not in conflict with the Comprehensive Plan, as the proposed lots are Code-compliant and will serve development that is consistent with the South Loop Mixed Use land use category.

**2. The plat is not in conflict with any adopted District Plan for the area;**

- The proposed plat is consistent with the adopted South Loop District Plan.

**3. The plat is not in conflict with City Code Provisions;**

- The proposed plat is not in conflict with any provisions of the City Code.

**4. The plat does not conflict with existing easements;**

- There are no known easements in conflict with the plat.

**5. There is adequate public infrastructure to support the additional development potential created by the plat;**

- The development proposed on the site is adequately served by public infrastructure. Additional private roadways will be constructed to facilitate travel to the parking structure.

**6. The plat design mitigates potential negative impacts on the environment, including but not limited to topography; steep slopes; trees; vegetation; naturally occurring lakes, ponds, rivers, and streams; susceptibility of the site to erosion, sedimentation or flooding; drainage; and stormwater storage needs;**

- The plat must establish new drainage and utility easements. The site is presently used for surface parking. The subject area is currently served by City utilities and stormwater management facilities. No negative impacts on the environment will occur as a result of the proposed plat.

**7. The plat will not be detrimental to the public health, safety and welfare; and**

- The plat subdivides one outlot into one buildable lot and three outlots. The platted lot will be utilized to construct a parking structure that is consistent with approved development plans for the Bloomington Central Station development. The proposed plat will not be detrimental to the public health, safety, or general welfare.

**8. The plat is not in conflict with an approved development plan or plat.**

- The proposed plat is not in conflict with the existing plat, Bloomington Central station 2<sup>nd</sup> Addition, or the approved Preliminary Development Plan for Bloomington Central Station.

**Section 22.06(d)(1) Final Plat**

**1. The plat is not in conflict with the approved preliminary plat or the preliminary plat findings.**

- The Final Plat is consistent with the Preliminary Plat and the Preliminary Plat findings.

**RECOMMENDATION**

Staff recommends approval of the Preliminary and Final Plat and Major Revision to Final Development Plans through the following motions:

In Case PL2016-146, having been able to make the required findings, I move to recommend approval of a Preliminary and Final Plat for Bloomington Central Station 5<sup>th</sup> Addition, subject to the conditions and Code requirements attached to the staff report.

In Case PL2016-146, having been able to make the required findings, I move to recommend approval of a Major Revision to Final Development Plans to construct a 1,666 stall parking structure and surface parking improvements at 8170 31<sup>st</sup> Avenue South, subject to the conditions and Code requirements attached to the staff report.

## **RECOMMENDED CONDITIONS OF APPROVAL**

**Case** PL201600146

**Project Description:** Type II Preliminary and Final Plat to divide one outlot into one lot and three outlots; Major Revision to Final Development Plans for a new 8-level parking structure plus loading dock and surface parking modifications. Airport Zoning Permit.

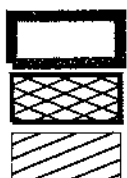
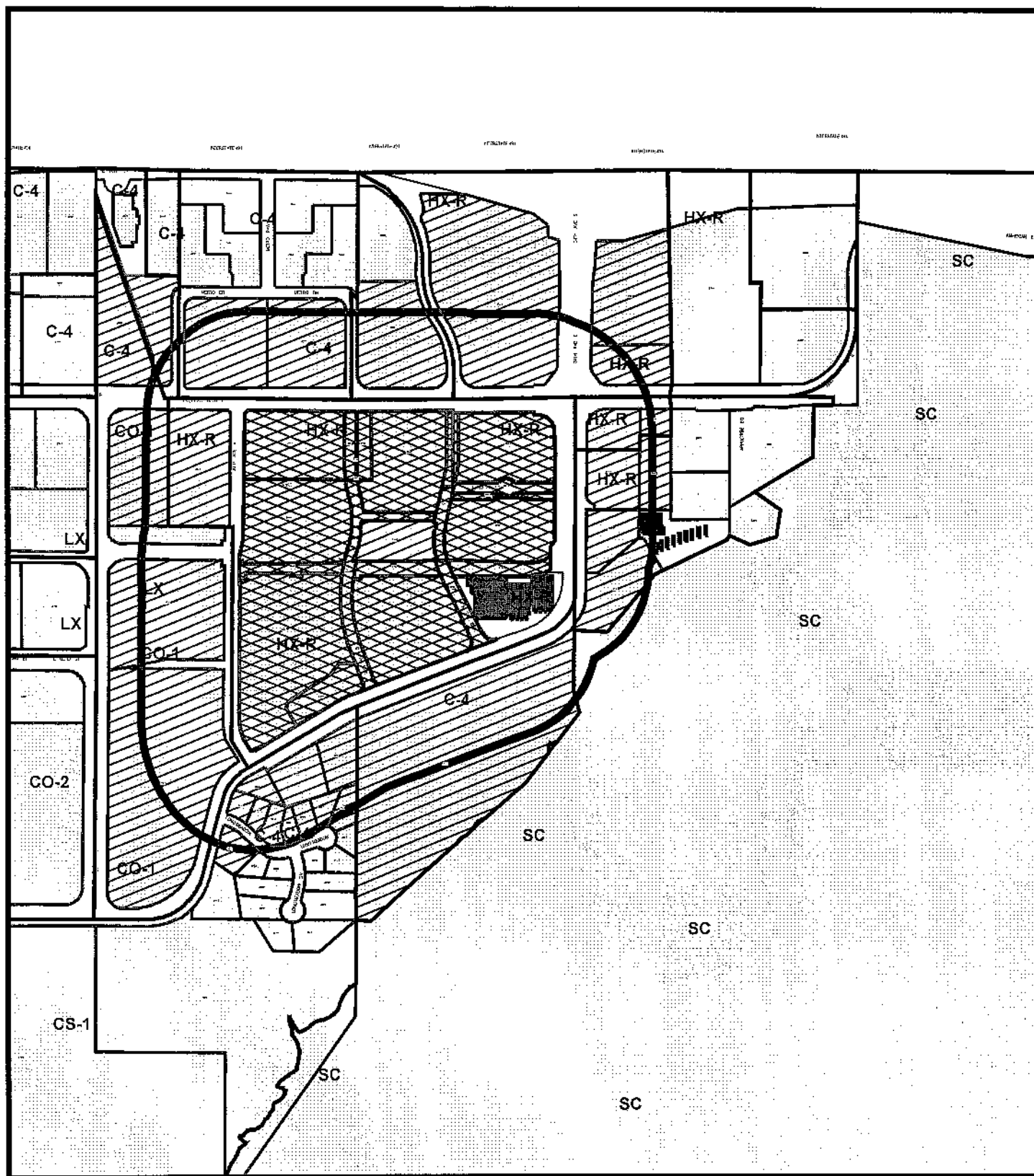
**Address:** 8170 31ST AVE S

The following conditions of approval are arranged according to when they must be satisfied. In addition to conditions of approval, the use and improvements must also comply with all applicable local, state, and federal codes. Codes to which the applicant should pay particular attention are included below.

1. Prior to Permit The property must be platted per Chapter 22 of the City Code and the approved final plat must be filed with Hennepin County prior to the issuance of any permits (22.03(a)(2)).
2. Prior to Permit The Grading, Drainage, Utility, Erosion Control, Access, Traffic, Parking and Circulation plans must be approved by the City Engineer.
3. Prior to Permit Sewer Availability Charges (SAC) must be satisfied.
4. Prior to Permit A Minnesota Pollution Control Agency (MPCA) Sanitary Sewer Extension or Modification Permit must be obtained or notification from the MPCA that this permit is not required must be submitted to the City (State of MN).
5. Prior to Permit A Minnesota Department of Health (MDH) watermain review and approval must be obtained or notification from MDH that this permit is not required must be submitted to the City (State of MN).
6. Prior to Permit Utility plan showing location of existing and proposed water main and fire hydrant locations must be approved by the Fire Marshal and Utilities Engineer (City Code Sec. 6.20, Minnesota State Fire Code Sec. 508).
7. Prior to Permit Building must be provided with an automatic fire sprinkler system as approved by the Fire Marshal (MN Bldg. Code Sec. 903, MN.Rules Chapter 1306; MN State Fire Code Sec. 903).
8. Prior to Permit A National Pollutant Discharge Elimination System (NPDES) construction site permit and a Storm Water Pollution Prevention Plan (SWPPP) must be provided if greater than one acre is disturbed (State of MN and Federal regulation).
9. Prior to Permit Storm Water Management Plan must be provided that demonstrates compliance with the City's Comprehensive Surface Water Management Plan. A maintenance plan must be signed by the property owners and must be filed of record with Hennepin County.
10. Prior to Permit Public drainage and utility easement must be provided on the plat as approved by the City Engineer and proof of filing must be provided to the Manager of Building and Inspection:
11. Prior to Permit Private common driveway/access and utility easement agreements be filed with Hennepin County and proof of filing provided to the Engineering Division

12. Prior to Permit A 10-foot sidewalk/bikeway easement must be provided by document along all street frontages as approved by the City Engineer.
13. Prior to Permit Sidewalk must be installed the west side of 31st Avenue South connecting to the south sidewalk of E 82nd Street. All sidewalks within the development must meet Section 21.301.04 of the City Code.
14. Prior to Permit Bicycle parking spaces must be provided and located throughout the site as approved by the City Engineer.
15. Prior to Permit An erosion control surety must be provided (16.05(b)).
16. Prior to Permit Landscape plan must be approved by the Planning Manager and landscape surety must be filed (Sec 19.52).
17. Prior to Permit Parking lot and site security lighting plans must be revised to satisfy the requirements of Section 21.301.07 of the City Code.
18. Prior to Permit Exterior building materials must be approved by the Planning Manager (Sec. 19.63.08).
19. Prior to Permit An Airport Zoning Permit must be approved by the Community Development Director for any crane or structure on site that exceeds 60 feet in height above existing grade (MSP Airport Zoning Ordinance Section VIII (A)).
20. Prior to Permit Haul Route and Construction Traffic Control Plans must be approved by the City Engineer.
21. Prior to Permit A Construction Management Plan must be submitted for review and approval by the City
22. Prior to C/O The developer must submit electronic utility as-builts to the Public Works Department prior to the issuance of the Certificate of Occupancy.
23. Prior to C/O Prior to occupancy, life safety requirements must be reviewed and approved by the Fire Marshal.
24. Ongoing The site and building improvements are limited to those shown on the approved plans in Case File #PL201600146.
25. Ongoing Alterations to utilities must be at the developer's expense.
26. Ongoing All construction stockpiling, staging, loading, unloading, drop-off, pick-up and parking must take place on-site and off adjacent public streets and public rights-of-way.
27. Ongoing Development must comply with the Minnesota State Accessibility Code.
28. Ongoing Fire lanes must be posted as approved by the Fire Marshal (MN State Fire Code Sec. 503.3).
29. Ongoing Three foot high parking lot screening must be provided along 30th Avenue South as approved by the Planning Manager (Sec. 19.52).
30. Ongoing Poured-in-place concrete curbs must be provided on the perimeter of parking lots and traffic islands (Sec 19.64).

# City of Bloomington Notification Map

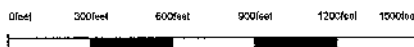


Notification Boundary  
Applicant Property  
Notified Properties

Zoning District Boundary  
(Labels Refer to Zoning District)

PL2016-146

(A notice of this application was sent to the registered owner of these properties)



Scale: 1:8400

Plot time: 08/26/2016 14:42:25

# AFFIDAVIT OF PUBLICATION

STATE OF MINNESOTA ) ss  
COUNTY OF HENNEPIN

Charlene Vold being duly sworn on an oath, states or affirms that he/she is the Publisher's Designated Agent of the newspaper(s) known as:

SC Bloomington

with the known office of issue being located in the county of:

HENNEPIN

with additional circulation in the counties of:  
HENNEPIN

and has full knowledge of the facts stated below:

(A) The newspaper has complied with all of the requirements constituting qualification as a qualified newspaper as provided by Minn. Stat. §331A.02.

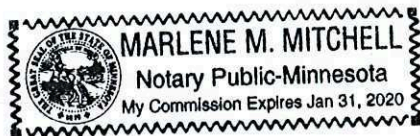
(B) This Public Notice was printed and published in said newspaper(s) once each week, for 1 successive week(s); the first insertion being on 09/15/2016 and the last insertion being on 09/15/2016.

**MORTGAGE FORECLOSURE NOTICES**  
Pursuant to Minnesota Stat. §580.033 relating to the publication of mortgage foreclosure notices: The newspaper complies with the conditions described in §580.033, subd. 1, clause (1) or (2). If the newspaper's known office of issue is located in a county adjoining the county where the mortgaged premises or some part of the mortgaged premises described in the notice are located, a substantial portion of the newspaper's circulation is in the latter county.

By: Charlene Vold  
Designated Agent

Subscribed and sworn to or affirmed before me on 09/15/2016 by Charlene Vold.

Marlene M. Mitchell  
Notary Public



## Rate Information:

(1) Lowest classified rate paid by commercial users for comparable space:  
\$34.45 per column inch

Ad ID 594475

## CITY OF BLOOMINGTON NOTICE OF PUBLIC HEARING BY THE PLANNING COMMISSION

CASE FILE NUMBER:

PL2016-146

APPLICANT: Bloomington Central Station LLC

PROPERTY ADDRESSES:

8170 31st Avenue South; 8170 33rd Avenue South; 3098 East Old Shakopee Road; and 8151 30th Avenue South

PROPOSALS: Type II Preliminary and Final Plat to divide one outlot into one lot and three outlots; Major Revision to Final Development Plans for a new 8-level parking structure plus loading dock and surface parking modifications.

DATE, LOCATION, AND TIME OF HEARING:

09/29/2016, 6:05 p.m.

City Council Chambers -

Bloomington City Hall

1800 West Old Shakopee Road

City Council consideration date is announced at the Planning Commission meeting date at which action is taken.

HOW YOU CAN PARTICIPATE:

(Please include Case File number above when corresponding)

1. Submit a letter to the address below expressing your views;  
2. Attend the hearing and give testimony about the proposal; and/or

3. Contact the Planning Division using the information below.

FURTHER INFORMATION:

Nick Johnson, Planner

1800 West Old Shakopee Road

Bloomington, MN 55431-3027

Phone: 952-563-8925

Email:

nmjohnson@BloomingtonMN.gov

Published in the  
Bloomington Sun Current  
September 15, 2016  
594475

# **BCS HealthPartners Parking Ramp Development Application**

## **Major Revision to Final Development Plan Preliminary and Final Plat Airport Zoning Permit**



### **Project Narrative**

August 24, 2016 (Rev. 2)

**Property Owner:**

Bloomington Central Station LLC  
c/o McGough Development  
2737 Fairview Avenue North  
St. Paul, MN 55113

**Developer:**

McGough Development  
2737 Fairview Avenue North  
St. Paul, MN 55113

**Prepared by:**

Kimley-Horn and Associates, Inc  
Oslund and Associates  
BWBR Architects  
Hunt Electric

## A. DEVELOPMENT APPLICATION REQUESTED ACTIONS

The requested actions for the BCS HealthPartners Parking Ramp Project Development Application will be as follows:

- Major Revision to Final Development Plan for Bloomington Central Station
- Final Site Plan and Building Plan for BCS HealthPartners Parking Ramp on Lot 1, Block 1, and Outlot A, Bloomington Central Station 2<sup>nd</sup> Addition
- Preliminary and Final Plat Approval for Bloomington Central Station 5<sup>th</sup> Addition (subdivision of Outlot A, Bloomington Central Station 2<sup>nd</sup> Addition) – one (1) lot and three (3) outlots
- Airport Zoning Permit

The Development Application for the BCS HealthPartners Parking Ramp Project will adhere to the following proposed approval schedule:

|   |                    |
|---|--------------------|
| Informal Development Review Committee (DRC) Submittal (completed) | December 3, 2015   |
| Informal DRC Meeting (completed)                                  | December 8, 2015   |
| Submit Development Application to the City                        | August 24, 2016    |
| Formal DRC Meeting  | September 13, 2016 |
| Planning Commission Meeting                                       | September 26, 2016 |
| City Council Meeting  | October 24, 2016   |

The Development Application will include the following:

- Development Application
  - Compact disk containing copies of all hard copy items
  - Project Narrative
  - Updated BCS Stormwater Management Summary
  - Site Development Plans:
    - Four (4) full size sets (three folded and one rolled)
    - Four (4) sets of 11 x 17
  - Development Application
    - Major Revision to Final Development Plan \$ 830
    - Preliminary Plat – Type II (\$700 plus \$90 per lot) \$1,060
    - Final Plat – Type II (\$400 plus \$20 per lot) \$ 480
- Total Application Fees \$2,370

The Development Application also includes Bloomington Central Station Ramp Traffic Study prepared by SRF Consulting Group, Inc., dated June 16, 2016 (under a direct contract with the City of Bloomington). A Tier 1 Travel Demand Management Plan (TDMP) will not be prepared for this project.



## **B. PROJECT LOCATION**

The project site is located south and west of the current HealthPartners building located at 8170 33<sup>rd</sup> Avenue South, in Bloomington, MN. The site is bounded on the south by East Old Shakopee Road; on the west by 30<sup>th</sup> Avenue South; on the north by the Blue Line LRT corridor; and on the east by the existing HealthPartners building. The site is primarily surface parking lots serving HealthPartners.

## **C. PROPERTY**

The HealthPartners Parking Ramp will be built on what is currently platted as Outlot A, Bloomington Central Station 2nd Addition, Hennepin County, Minnesota. Outlot A has a PIN Number 01-027-24-14-0016 and is owned by Bloomington Central Station LLC.

Outlot A will be subdivided by Bloomington Central Station 5<sup>th</sup> Addition into one lot and three outlots as follows:

|               |            |  |
|---------------|------------|--|
| Lot 1 Block 1 | 90,600 SF  | HealthPartners Parking Ramp – Phase I  |
| Outlot A      | 120,885 SF | West Parking Lot – Future HealthPartners Expansion<br>Building and Parking Ramp Phase II |
| Outlot B      | 29,916 SF  | East 82 <sup>nd</sup> Street   |
| Outlot C      | 124,307 SF | Remaining Southwest Parking Lot – Future<br>Development Lot                              |
| Total         | 365,708 SF |  |

Refer to the submitted BCS 5<sup>th</sup> Addition Preliminary and Final Plat. Note that when the parking ramp is expanded in the future, the expansion will occur on a future 35,600  $\pm$  SF lot or Lot 1 Block 1 will be replatted to include the expansion.

The HealthPartners Loading Dock and South Parking Lot improvements will be made on Lot 1, Block 1, Bloomington Central Station 2nd Addition. No changes are proposed to that lot. Other improvements will be made to other property at Bloomington Central Station including Outlot H, and Outlot J, Bloomington Central Station 2nd Addition.

## **D. PROPOSED PROJECT**

The project consists of the design and construction a new HealthPartners parking ramp, partial renovation of the existing 8170 building and site improvements south of the existing light rail.

### **Existing HealthPartners Building – 8170 33<sup>rd</sup> Avenue South**

The existing building will receive upgraded mechanical/electrical infrastructure. The existing loading dock will be renovated and include a revised entry sequence to allow for the construction of a portion of 31<sup>st</sup> Avenue South, which will run north to south to a new vehicle drop, surface guest parking and access to the new parking ramp and other at surface parking. The existing south parking lot will be modified to allow for the revised loading access. The entry level at grade will receive minor modifications including a new ADA compliant set of exterior doors and a pedestrian bridge from the South Parking Lot, over the new loading access to the new set of exterior doors. The bridge is planned to exceed exit width requirements serving the south exit and

adjacent plaza area. The interior of the existing building will receive finish upgrades in the lower level elevator lobby. All of the original bathroom in the existing building will be fully remodeled to be ADA compliant.

The revised loading dock will have two new overhead doors at 14'-0" high by 25'-0" wide on the south face of the existing building. The existing site will be excavated on to the south side of the building to the level of the existing loading dock. The existing wall of the building will be exposed cast in place concrete that will be sandblasted after the new doors have been cut into the existing basement wall. Plantings are planned for the new sloped surface along with some trees to screen views toward the new loading dock drive. The interior of the revised loading dock is intended to accommodate a full semi-tractor trailer within the dock with the doors closed. The dock will also include space for two additional box trucks, a trash compacter and a dumpster. Garbage and recycling bins will be stored within the dock and moved to the exterior for material removal.

The South Parking Lot will be removed and redesigned. It will accommodate 63 parking spaces, including three accessible parking spaces. It will be graded such that it could accommodate all the required accessible parking currently being proposed in the West Parking Lot when the future HealthPartners Expansion Building is constructed. An 8-foot wide steel truss pedestrian bridge will connect the South Parking Lot to the southwest building plaza and south entry. The exit width requirement for this south entry is 65".

West of the existing HealthPartners building and south of the HealthPartners Parking Ramp, a new surface parking lot will be constructed. The West Parking Lot is located where the future HealthPartners Expansion building will be built. A portion of this lot will include visitor parking and all of the required accessible parking spaces for the surface Southwest and West Parking Lots. A raised pedestrian connection between the existing building and parking ramp vestibule will be construction. This will be able to accommodate a canopy and wind screen that is not a part of this project. Improved employee drop-off areas are included in the project. The West Parking Lot will also reconstruct the accessible ramp to the existing west building entrance.

### **HealthPartners Parking Ramp**

The new parking ramp is seven levels above grade with one level below. The ramp supports 1666 vehicles with a footprint of roughly 275' east to west by 245' north to south. The ramp and associated surface parking south of the light rail provide all of the necessary parking for the existing 8170 building. The ramp will have two-way traffic with two sloped parking ramps connecting levels. The ramp will have a precast structure with precast spandrel panels. The ramp is designed to accommodate another two full bays of parking in the future to accommodate the additional parking need if an additional office building is built on the parcel between the ramp and the existing 8170 building.

The new ramp will be clad in a warm gray precast similar in tone to the existing 8170 building and other developments on the Bloomington Central Station campus. The finish is planned to be a combination of sandblasted and acid etched finishes to create pattern. This pattern, along with added reveals and relief within certain panels, are intended to animate the façade of the ramp. On the south and north side of the ramp the design includes exposing a unique white precast structure that breaks up the overall mass of the ramp. On the east side of the new parking ramp a stair and elevator tower will have enclosed elevator lobbies and glass back elevators with the eastern face of the elevator tower clad in glass allowing visibility into the elevators at all times from the exterior.

The site is intended to be landscaped in a manner consistent with the overall Bloomington Central Station campus. Patterned concrete, a mix of plantings and the use of berms will assist in wayfinding within the campus and provide screening for some of the parked cars. The sidewalk at the south side of the light rail will be finished in a similar fashion as the area to the north of the existing 8170 building and the Reflections project to the east.

A snow drop is located at the southeast corner of the parking ramp. Snow will be dumped from the seventh level of the ramp onto a concrete pad. The snow will be hauled offsite.

### **Future HealthPartners Expansion Building**

The future HealthPartners Expansion Building has been considered and planned for in the revised Preliminary Development Plan and the Final Development Plans for the HealthPartners Parking Ramp Project. The future building will be an eight-story office building of roughly 261,000 gross SF. There will be one level below grade consisting of mechanical and electrical, storage, and a small amount of office space. The building will have a building height of 136'-8". The new building will serve as the main entry to the HealthPartners campus and may include an auto court to accommodate short-term visitors. There is a one-story connection from the proposed building to the adjacent HealthPartners Parking Ramp elevator/stair tower. This one-story connection will also include a new loading dock, capable of serving two semi-trailers and one box truck. A skyway is proposed from the 8170 building to the new building.

### **Existing Surface Parking Modifications**

The remaining Southwest Parking Lot, south of East 82<sup>nd</sup> Street, will be improved. The entrance at the southwest corner will be improved with concrete curb and gutter and connect to the curb cut built with the City of Bloomington's 30<sup>th</sup> Avenue South improvements. A new concrete drive way will be constructed at the northeast corner of the ramp at East 82<sup>nd</sup> Street and portion of the lot will be reconstructed for to address pavement grades. New concrete curb and gutter will replace existing bituminous curb installed over 10 years ago. The remainder of the parking lot will be milled, overlaid, and restriped. New parking lot lighting is proposed. These improvements are considered maintenance improvements to extend the life of the existing parking lot until it can be developed in the future.

### **Roadway Improvements**

The project will include the construction of several roadways. These roadways will be constructed by McGough Development to City of Bloomington standards. The roadway will be encumbered by a public roadway easement.

East 82<sup>nd</sup> Street will connect 30<sup>th</sup> Avenue South with 31<sup>st</sup> Avenue South, south of the proposed HealthPartners Parking Ramp. The roadway will be a 30-foot wide, two-way roadway with streetscape and lighting similar to East 80½ Street (BCS Phase 2B). The roadway will have 12" DIP water main connecting 30<sup>th</sup> Avenue South water main with 31<sup>st</sup> Avenue South water main, and provide hydrants and water services for the future buildings. The roadway will have storm sewer for both the roadway as well as the HealthPartners parking ramp and future building. The roadway will have 8" PVC sanitary sewer that will serve the HealthPartners parking ramp as well as the future building and parking ramp in the southwest corner of the BCS development.

31<sup>st</sup> Avenue South will be constructed from East Old Shakopee Road to a point north of East 82<sup>nd</sup> Street. The roadway will have varying typical sections, but generally will be a 38-foot wide, two-way roadway with parking. The streetscape and lighting is designed to match 33<sup>rd</sup> Avenue South

(BCS Phase 2A), but will install only trees, street lighting, and only gray sidewalk on the east side. The additional hardscape and planting areas will be deferred until the future HealthPartners Building is built and the roadway is extended north of the Blue Line LRT. Storm sewer and water main was installed in 2006 anticipating this roadway construction. A 10" and 8" PVC sanitary sewer is proposed in the segment of 31<sup>st</sup> Avenue South north of the Blue Line LRT to serve the future buildings in the northwest corner of the BCS development.

The Blue Line LRT pedestrian corridor will be construction north of the HealthPartners Parking Ramp and south of the LRT track curb. The streetscape and lighting will be similar to the pedestrian corridor north of Reflections. The corridor will consist of two 8-foot trails separated by a 4-foot planting area. This corridor will be able to accommodate fire trucks, and will include depressed curb at 30<sup>th</sup> Avenue South and near 31<sup>st</sup> Avenue South. The corridor will include 8" DIP water main connecting 30th Avenue South water main with 31st Avenue South water main. The corridor will provide hydrants and the Parking Ramp combined fire protection and domestic water service. The corridor will also include 12" PVC sanitary sewer that will serve the future building, as well as the future buildings in the northwest corner of the BCS development. A future crossing under the Blue Line LRT tracks will be required.

The recently completed 30<sup>th</sup> Avenue South streetscape improvements will be impacted by the HealthPartners parking ramp construction. Sheet piling or other soil retention system will be installed at the back of the easterly curb. Street trees; plantings; irrigation main and heads; traffic control conduit and hand holes; light foundations, conduit, and hand holes; sidewalks; and signage will be removed along the west side the parking ramp to construct the lower level. The street light and pedestrian lights will be salvaged and reinstalled. These streetscape improvements will be replaced.

### **Site Landscaping**

The landscape design of the BCS HealthPartners Parking Ramp and Loading Dock Project includes the following:

- Parking Lot Screening along East Old Shakopee Road and 30th Avenue South, which is accomplished by using existing planting materials and new trees and perennial plantings.
- Landscaping with shrub plantings, evergreen and deciduous trees to provide landscape coverage over the 8170 loading dock embankment.
- Plantings that include deciduous trees and perennials to line 31st Avenue South, which matches typical planting scheme on other parts of the Bloomington Central Station Development. The streetscape and planting beds will be installed in the future. Structure soil will be installed in continuous trenches along 31<sup>st</sup> Avenue South
- Plantings that include deciduous trees and perennials, along with rock mulch, to line 82nd Street.
- Rhythmic trees, shrubs, perennials and rock mulch to line Blue Line LRT pedestrian corridor and to match master plan design intent.
- A planting berm to screen parking lot views from 31st Avenue South. The berm will include perennials plantings and turf grass sod.
- Foundation landscape plantings around new parking ramp, which includes deciduous trees, perennials, shrubs, rock mulch, and turf grass sod.
- Trees comply with city code section 19.52(c) (2) (A).
- Shrubs/perennials comply with city code section 19.52(c) (2) (B).
- Trees and perennial plantings along 30<sup>th</sup> Avenue South will be removed and reinstalled to complete the parking ramp construction. Existing irrigation will be removed and reinstalled.

- A complete irrigation system, with a water source located in the northeast corner of the parking ramp, will be installed consistent with the rest of the BCS campus.

## **Site Lighting**

Site lighting will adhere to City Code Sec. 21.301.07. Two separate photometric plans superimposed on a site plan have been prepared: one plan with the initial at-grade foot candle levels; and the other the maintained at-grade foot candle levels. Site photometric, fixture cuts/specification, and energy calculations are provided on the associated submitted drawings. Lighting controls will adhere to current energy codes and to include daylighting and motion sensing individual fixtures in the ramp as well as daylighting and dimming for the surface lots.

Upper level (top deck) of the HealthPartners parking ramp will utilize fixtures on a 25' pole on a 4' base. The maximum overall height based on FAA requirements to be 32'. Photometrics for the upper level has been included with the overall site photometrics. Photometrics for the lower and typical floors for the ramp to be submitted at a future date.

In the South Parking Lot and loading dock area, the existing light poles will be salvaged and reinstalled based on the reconfiguration of the parking area. These fixtures will also be retrofitted with new LED luminaires. The new dock area and adjacent walkway will be lit utilizing new poles and LED fixtures as shown on the site plan.

In the Southwest Parking Lot and West Parking Lot, the existing lighting will be removed and replaced with new poles and LED fixtures based on the new parking layout. These fixtures will match those light fixtures at the adjacent BCS Hyatt Regency in color and height.

Along 31st Avenue South and East 82nd Street, the street lights will match those fixtures and poles currently installed on 33rd Avenue South and East 80½ Street. They will be located to accommodate permanent streetscape design to be installed in the future. Weatherproof GFI convenience receptacles to be located at every other tree for tree/holiday lighting.

Along the Blue Line LRT pedestrian corridor, the fixtures, poles, and precast concrete bases to match those located along the Blue Line LRT corridor and Central Station Park north of the existing HealthPartners building. Weatherproof GFI convenience receptacles to be located at every other tree for tree/holiday lighting.

Along 30th Avenue South, the existing street lights and pedestrian lights that are impacted by construction will be salvaged and reinstalled. The existing fixtures have been reflected on the site photometric plan to illustrate the impact on the overall site.

## **E. ZONING CODE ANALYSIS**

### **1. Comprehensive Plan**

The HealthPartners Parking Ramp project site is within the South Loop District of the City of Bloomington. The current Land Use Guide Plan designates the project site as South Loop Mixed Uses (SLMU). The South Loop Mixed use designation is designed to work with the HX-R Zoning District and office land use is consistent with the Comprehensive Plan. No Comprehensive Plan changes are proposed with this application.

## **2. Zoning**

The entire Bloomington Central Station redevelopment site is zoned High Intensity Mixed Use with Residential HX-R Zoning District (Planned Development). The intent of this district is to provide for high intensity employment-oriented, tourist-oriented and residential uses in areas close to frequent transit services. The proposed parking ramp will support the existing and future office use., which is a permitted principal use in the HX-R District. No zoning changes are proposed with this application.

## **3. Airport Zoning**

The City of Bloomington adopted the Airport Runway (AR-17) Overlay District that codifies the 2004 MSP Zoning Ordinance. This ordinance creates Safety Zone A (RPZ), Safety Zone B, and Safety Zone C for Runway 17-35, which creates additional zoning requirements for this project site.

The southwest corner of HealthPartners Parking Ramp and Lot 1, Block 1 Bloomington Central Station 5<sup>th</sup> Addition falls within Safety Zone B. The remainder of the lot is in Safety Zone C and is in the Transition Surface and Zone. The maximum object elevation transitions from an approximately elevation of 912 (NGVD 1929) at the southwest corner to an approximate elevation of 955 at the northeast corner of the parking ramp. The proposed parking ramp building height is 69'-0" at the top of the spandrel walls, and with a proposed finish floor elevation of 816.50, it has a maximum elevation of 885.50. The proposed parking ramp elevator tower roof height is 81'-6", and with a proposed finish floor elevation of 816.50, it has a maximum elevation of 898.00. These are below the Precision Instrument Approach Surface of Safety Zone B and below Transition Surface of Safety Zone C.

The MSP Zoning Ordinance establishes the maximum construction height of 80 feet for the entire development parcel, before requiring an Airport Zoning Permit. Consistent with City Code Section 19.38.03, an Airport Zoning Permit from the City of Bloomington will be required for the parking elevator tower, as well as for the mobile crane that will construct this parking ramp. The project will require an Airport Zoning Permit for the parking ramp and mobile crane and is being sought with this application.

Well Precast provide their mobile crane plan for the erection of the HealthPartners Parking Ramp. Two Manitowoc 2250 Series 3 mobile cranes with a 220-foot boom will be used to set columns, beams, double tees, and spandrel panels. They will erect from west to east and will not be operating in 30<sup>th</sup> Avenue South. The maximum height of the crane will be 226 feet from the lower level of the parking ramp (elevation 805.83), or an elevation of 1031.8 along the west side of the parking ramp. This elevation rises to 1042.5 along the east side of the parking ramp because the crane will operate at existing grades. This rectangle volume of crane operation area exceeds the Part 77 surfaces (Precision Instrument Approach Surface and the Transition Surface) which will require MSP Zoning Ordinance Variance from the MAC MSP Zoning Board of Adjustment.

An FAA 7460-1 Airspace Study of the parking ramp and mobile crane will be required based on proximity to MSP International Airport. The Determinations of parking ramp points and mobile crane working areas will be issued by the Federal Aviation Administration. It is anticipated that the parking ramp points will likely result in a no hazard determination. The mobile crane working area determinations will place operational restrictions and demarcation requirements. It is anticipated that the crane boom will be required to be lowered from sun down until sun up. Any penetration into the Part 77 surface, even temporary construction penetrations, will require an

MSP Zoning Ordinance Variance from the MAC MSP Zoning Board of Adjustment. This process would occur after the FAA determinations.

The City will not issue the Airport Zoning Permit until the Federal Aviation Administration (FAA) and the Metropolitan Airports Commission (MAC) approved the project.

#### **4. Dimensional Requirements**

City Code Section 19.29(h)(1) (HX-R District) requires no minimum setback requirement from property lines fronting public streets 30<sup>th</sup> Avenue South. There is a 10-foot drainage and utility easement proposed and a 10-foot existing sidewalk and bikeway easement along 30<sup>th</sup> Avenue South. The parking ramp has been positioned so that no encroachment into these easements, including the foundations, will be required. The parking entry canopy has not been design and may require an encroachment into the drainage and utility and existing sidewalk and bikeway easement at or above the Second Level of the parking ramp.

City Code Section 19.29(h) (2) (HX-R District) requires a minimum site area of 120,000 SF. The site area for Lot 1, Block 1 of Bloomington Central Station 5<sup>th</sup> addition will be 90,600 SF. For parcels with areas below 120,000 SF, they may be created as part of a planned development, provided the area of the overall planned development is at least 120,000 square feet.

#### **5. Building Height**

City Code Section 19.29(h) (3) (HX-R District) refers to City Code Section 21.301.10 for maximum structure height. According to the Bloomington Building Heights Limits Map, this site is restricted only by the Airport Zoning height limits, as discussed above. The proposed parking ramp height is 80'-6".

City Code Section 21.301.10(e) establishes Pedestrian Street Step Back Standards. 31<sup>st</sup> Avenue South is designated as a Pedestrian Street Segment. The height of any portion of a new building adjacent to a Pedestrian Street Segment may not exceed the horizontal distance of that portion of the structure to the centerline of the adjacent Pedestrian Street Segment, except that portions of buildings or structures more than 80 feet from the centerline are exempt from further step back.

#### **6. Parking**

City Code Section 19.29(i) (HX-R District) requires that required parking be located below grade, within structured parking ramps, or be on-street. This code prohibits surface parking, allowing only a small number of visitor surface parking spaces. The surface parking lots proposed with this Development Application are temporary and will be replaced with below grade and structured parking ramps in the future.

#### **7. Open Space and Landscaping**

City Code Section 19.29(l) (HX-R District) refers to City Code Section 19.52 for landscaping and screening requirements. City Code Section 19.52(c) (2) (A) requires one tree for every 2,500 SF of Developable Landscape Area. McGough Development intends to use trees within the East 82<sup>nd</sup> Street, 31<sup>st</sup> Avenue South, and Blue Line LRT pedestrian corridor streetscape to meet the tree requirements. The Developable Landscape Area is 306,193 SF. The required number of trees is 122. The number trees provided is 143.

City Code Section 19.52(c) (2) (B) requires one shrub for every 1,000 SF of Developable Landscape Area. The Developable Landscape Area is 306,193 SF. The required number of shrubs is 306. The number shrubs provided is 2,676.

City Code Section 19.52(d) requires perimeter screening of off-street parking areas and the public right-of-way. The screening must be between 3 and 4 feet, and can consist of plant material or berming, or a combination. This requirement will apply to a portion of the surface parking along 30<sup>th</sup> Avenue South, just north of East 82<sup>nd</sup> Street. No significant changes are proposed for screening of the reconfigured South Parking Lot or the existing Southwest Parking Lot.

## **10. Signage**

Sign regulations within the HX-R District must comply with Article X of Chapter 19. Parking g Ramp and site signage, other than regulatory-type signage, will be addressed through a Signage Permit process.

## **F. PARKING ANALYSIS**

Currently, HealthPartners employees park in surface parking lots both north and south of the Blue Line LRT corridor. As part of McGough's lease negotiation with HealthPartners, HealthPartners employees and visitors will park south of the Blue Line LRT corridor, in the surface parking lots and the new parking ramp. This frees up the area north of the Blue Line LRT corridor for redevelopment.

Section 21.301.06 of the City Code established parking and loading requirements. For office land uses, one parking space is required for every 285 SF of gross building area. The HealthPartners 8170 gross building area is 539,800 SF (not including the basement which is mostly parking). The code required parking spaces are 1,894 parking spaces.

The lease letter of intent with HealthPartners uses the leasable floor area of 470,891 SF and requires a total of 2,119 parking spaces, 225 parking spaces more than the code required.

The following is the parking provided:

|                        |                             |
|------------------------|-----------------------------|
| 8170 Executive Parking | 105 parking spaces          |
| South Parking Lot      | 63 parking spaces           |
| Southwest Parking Lot  | 172 parking spaces          |
| West Parking Spaces    | 277 parking spaces          |
| Parking Ramp           | <u>1,666 parking spaces</u> |
| Total                  | 2,283 parking spaces        |

The parking ramp has 129 parking spaces that are considered compact. This reduces the provided parking to 2,154 parking spaces for code comparison, which exceeds the code required parking by 260 parking spaces. Of the provided parking spaces, a total of 41 spaces are accessible, of which 28 are provided in the parking ramp.

With full expansion of the HealthPartners parking ramp, a total of 2,537 parking spaces are proposed, with 313 considered to be compact and 36 are accessible. The required parking will be calculated at that time.



## **G. TRAFFIC**

The City of Bloomington retained SRF Consulting Group, Inc. to prepare a traffic study for the Bloomington Central Station HealthPartners Parking Ramp Project. The primary purpose of that study was to look at the safety and traffic operations on the surrounding roadway network at opening and full build conditions. Included in the study was recommendations on the access point locations and access restrictions on 30<sup>th</sup> Avenue South.

The following is a list of recommendations from the study:

- The study recommends that the parking ramp (first and second phases) be limited to two access points. One lined up with the Metro Transit Park and Ride on 30<sup>th</sup> Avenue South and the other access from East 82<sup>nd</sup> Street. The current site plan locates the access points.
- Based on anticipate 2040 traffic forecasts, traffic signal and turn lane modifications are needed at East Old Shakopee Road / 30<sup>th</sup> Avenue South. The southwest office development parking ramp will need to divert 50% of the PM peak hour traffic away from this intersection to avoid congestion. This intersection will meet signal warrants when 60 to 70 percent of the proposed BCS land uses are developed.
- A sensitivity test indicated that if access control is added to the parking ramp, a southbound left turn queue on 30<sup>th</sup> Avenue South will back up north of the Blue Line LRT tracks. Newer access control technology may prevent this queueing from occurring.

The Bloomington Central Station Parking Ramp Traffic study was completed on June 16, 2016, and the recommendations included in the Final Development Plans.

A Tier 1 Transportation Demand Management Plan (TDMP) will be not be requires for this project. The City would like to see TDM strategies implemented for the existing 8170, but the financial guarantee is based only on a new building.

## **H. STORM WATER MANAGEMENT**

The proposed storm water management system has been designed in accordance with the BCS Overall Stormwater Management Plan, which ensures that the entire BCS development is meeting the requirements of all governing agencies, including the MPCA and the City of Bloomington Comprehensive Surface Water Management Plan. These criteria include water quality thresholds for total suspended solids and total phosphorus, as well as volume control and rate attenuation requirements. The Overall Stormwater Management Plan has been updated based for the current phase, and is included in the development application for consideration.

Given that soils are conducive to infiltration, these systems are designed to maximize the potential for infiltration to meet volume control and water quality requirements, and reduce the demand on the existing NURP pond located at the southwest corner of BCS. All infiltration devices have been designed in accordance with the MN Stormwater Manual design guidelines for infiltration as a best management practice.

The project can be generally split into two subwatersheds, each managing storm water in underground infiltration systems. The loading dock tributary runoff is managed by an underground perforated pipe chamber system, and due to site grading constraints, the outfall from the system cannot drain by gravity. To accommodate the 100-year event, some internal pumping is anticipated to attenuate the peak rate of discharge. Additional temporary storage will occur at the surface of the loading dock in the 100-year event, taking advantage of up to 6” of ponding

water adjacent to the inlet to the system. Following the peak of the storm, this ponding water will enter into the system and infiltrate. Runoff volumes in excess of the combined underground and surface storage will enter into an inlet near the building that will be directed into the building mechanical system and pumped to the roof drain leader.

For the watershed adjacent to the proposed parking ramp and surface parking, additional underground infiltration is proposed in the form of a perforated pipe infiltration gallery southeast of the proposed parking ramp. The outfall from this system will rely upon gravity storm sewer and a conventional outlet control structure design.

Refer to the updated BCS Stormwater Management Summary.

## **I. UTILITIES**

### **Sanitary Sewer**

The HealthPartners loading dock will require the reroute of the existing 10" sanitary sewer service on the south side of the building. The HealthPartners Parking Ramp will have two 8" services, one at the northwest corner and the other at the southwest corner. These are sized and located for future parking ramp expansion.

The future HealthPartners Expansion building has an 8" sanitary sewer stubbed from the 12" sanitary sewer lateral located north of the parking ramp in the Blue Line LRT pedestrian corridor.

### **Watermain**

The HealthPartners Parking Ramp will have an 8" combined domestic and fire protection water service into the northeast corner of the parking ramp. A 1½" domestic water meter will be located in the meter room for convenience water and irrigation. The water service is sized for the future parking ramp expansion. The parking ramp will not require a fire pump.

The future HealthPartners Expansion building has a 12" combined domestic and fire protection water service stubbed from the 12" water main located in East 82<sup>nd</sup> Street.

### **Private Utilities**

The electrical transformer will be located at the northeast corner of the parking ramp. Xcel Energy primary electrical cable will be run along the north side of the parking ramp from 30<sup>th</sup> Avenue South to the transformer. The electrical service will be sized with capacity for the parking ramp expansion. Emergency power for the parking ramp will be provided by a diesel emergency generator located in the northeast corner of the parking ramp. The emergency generator will be sized with capacity for the parking ramp expansion. No other private utilities are proposed for the parking ramp. Xcel Energy will be relocating a distribution line at the northwest corner of the parking ramp that was installed in error.

McGough Development and McGough Facilities Management have been working with communication companies about relocating communication lines along the south side of the existing HealthPartners buildings. The companies involved are Comcast, CenturyLink and Americom.

K:\TWO-LDE\MCGOUGH\BCS\DWG\PLAN SHEETS\HEALTH PARTNERS EXPANSION\CO.0.dwg August 24, 2016 12:28pm

REVISED FINAL DEVELOPMENT PLANS  
FOR  
BCS HEALTHPARTNERS PARKING RAMP  
S6, T27N, R23W  
BLOOMINGTON, HENNEPIN, MINNESOTA

ENGINEER  
KIMLEY-HORN AND ASSOCIATES, INC.

**Kimley»Horn**

PREPARED BY:  
2550 UNIVERSITY AVE W, SUITE 238 N  
ST. PAUL, MN 55114  
TELEPHONE (651) 645-4197

ARCHITECT  
BWBR ARCHITECTS  
380 ST. PETER STREET, SUITE 600  
ST. PAUL, MN 55102  
TELEPHONE (651) 222-3701

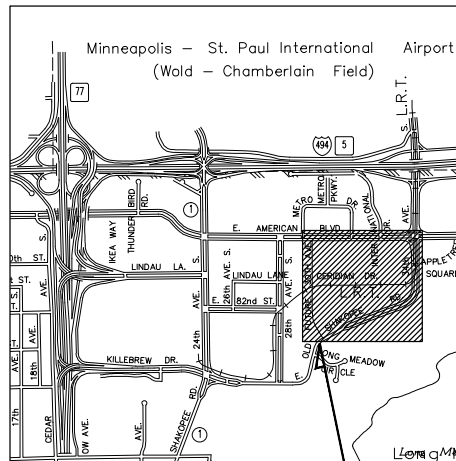
LANDSCAPE ARCHITECT  
OSLUND AND ASSOCIATES  
115 WASHINGTON AVE N, SUITE 200  
MINNEAPOLIS, MN 55401  
TELEPHONE (612) 359-9144

ELECTRICAL  
HUNT ELECTRIC CORPORATION  
7900 CHICAGO AVE S  
BLOOMINGTON, MN 55420  
TELEPHONE (651) 646-6575

OWNER / DEVELOPER  
MCGOUGH DEVELOPMENT



2737 FAIRVIEW AVENUE NORTH  
ST. PAUL, MN 55113  
TELEPHONE: (651) 634- 4687



VICINITY  
N.T.S.



SITE

NOTES:

1. CONTRACTOR SHALL CONFIRM THAT THE EXISTING CONDITIONS FOR THE SITE MATCH WHAT IS SHOWN ON THE DRAWINGS INCLUDED PRIOR TO CONSTRUCTION.
2. IF REPRODUCED, THE SCALES SHOWN ON THESE PLANS ARE BASED ON A 24" X 36" SHEET.
3. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICES COMPANIES SHALL BE PERFORMED PRIOR TO ANNOUNCED BUILDING POSSESSION AND THE FINAL CONNECTION OF SERVICES.
4. ALL GENERAL CONTRACTOR WORK TO BE COMPLETED (EARTHWORK, FINAL UTILITIES, AND FINAL GRADING) BY THE MILESTONE DATE IN PROJECT DOCUMENTS.



PRELIMINARY — NOT FOR CONSTRUCTION

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

THOMAS J. LINCOLN  
MN LIC. NO. 21433  
DATE:

DESIGNED BY:  
DRAWN BY:  
CHECKED BY:  
APPROVED:

TITLE SHEET

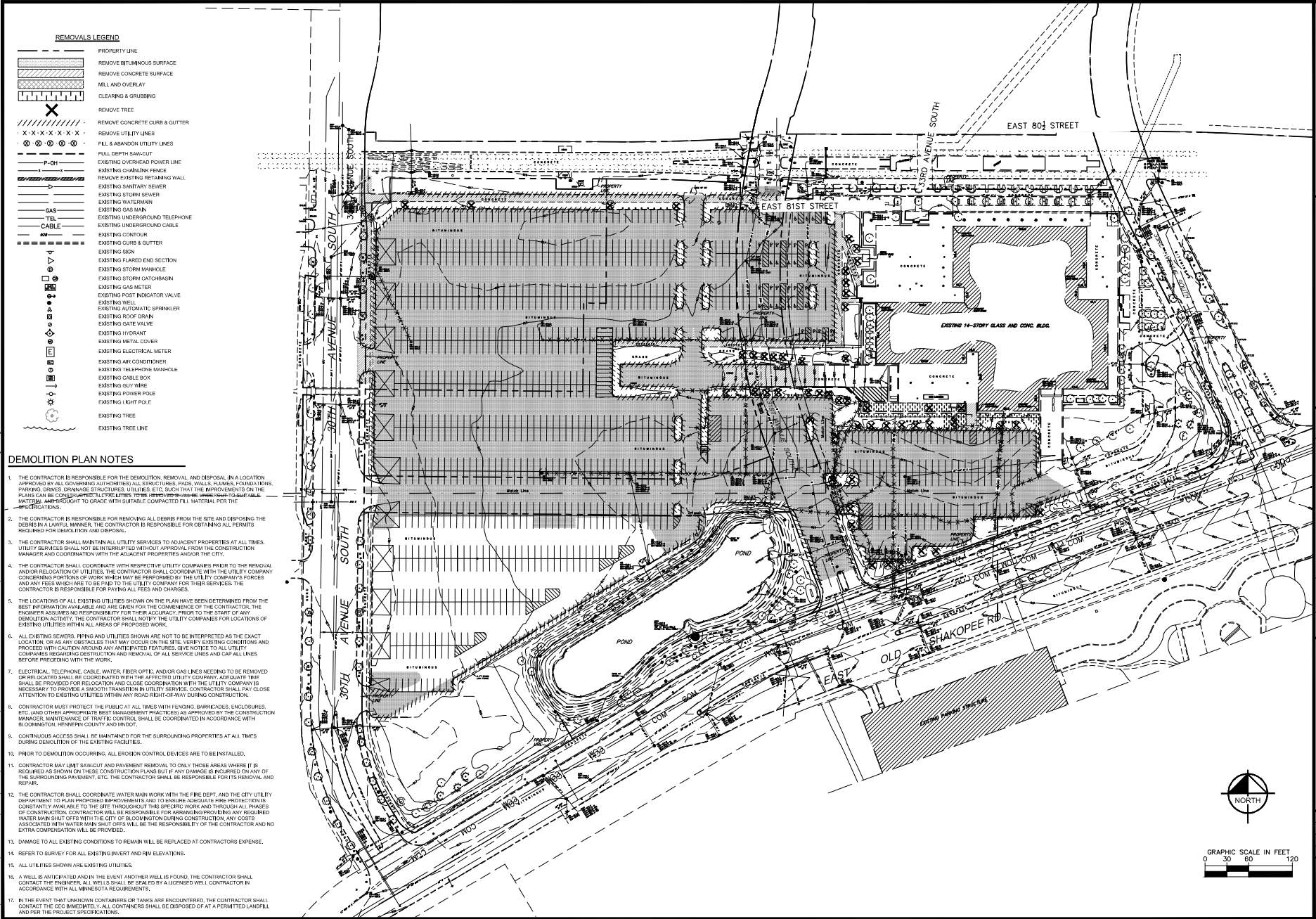
C0.0

BLOOMINGTON CENTRAL STATION  
HEALTHPARTNERS PARKING RAMP

Bloomington Central  
Station


Kimley»Horn






DEMOLITION PLAN NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL AND DISPOSAL, IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES, ALL STRUCTURES, PAVES, WALLS, FENCES, FOUNDATIONS, PARKING, DRIVES, DRIVEWAYS, STRUCTURES, UTILITIES, ETC. SUCH THAT THE IMPROVEMENTS ON THE PLANS CAN BE CONSTRUCTED. ALL UTILITIES TO BE REMOVED SHALL BE UNDERGROUND. THE CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE WITH SUBMITTAL CONTRACTOR FOR ALL MATERIALS FOR THE SPECIFICATIONS.
2. THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL DEBRIS FROM THE SITE AND DISPOSING OF THE DEBRIS IN A LAWFUL MANNER. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.
3. THE CONTRACTOR SHALL MAINTAIN ALL UTILITY SERVICES TO ADJACENT PROPERTIES AT ALL TIMES. UTILITY SERVICES SHALL NOT BE INTERRUPTED WITHOUT APPROVAL FROM THE UTILITY COMPANY. THE CONTRACTOR SHALL COORDINATE WITH THE ADJACENT PROPERTIES AND/OR THE CITY.
4. THE CONTRACTOR SHALL COORDINATE WITH RESPECTIVE UTILITY COMPANIES PRIOR TO THE REMOVAL AND/OR RELOCATION OF UTILITIES. THE CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY CONCERNING PORTIONS OF WORK WHICH MAY BE PERFORMED BY THE UTILITY COMPANY'S FORCES AND ANY FEES WHICH ARE TO BE PAID TO THE UTILITY COMPANY FOR THEIR SERVICES. THE CONTRACTOR IS RESPONSIBLE FOR PAYING ALL FEES AND CHARGES.
5. THE LOCATIONS OF ALL EXISTING UTILITIES SHOWN ON THE PLAN HAVE BEEN DETERMINED FROM THE BEST INFORMATION AVAILABLE AND ARE GIVEN FOR THE CONVENIENCE OF THE CONTRACTOR. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THEIR ACCURACY. PRIOR TO THE START OF ANY DEMOLITION ACTIVITY, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES FOR LOCATIONS OF EXISTING UTILITIES WITHIN ALL AREAS OF PROPOSED WORK.
6. ALL EXISTING SEWERS, PIPING AND UTILITIES SHOWN ARE NOT TO BE INTERPRETED AS THE EXACT LOCATION, OR AS ANY OBSTACLES THAT MAY OCCUR ON THE SITE. VERIFY EXISTING CONDITIONS AND PROCEED WITH CAUTION AROUND ANY ANTICIPATED FEATURES. GIVE NOTICE TO ALL UTILITY COMPANIES REGARDING DESTRUCTION AND REMOVAL OF ALL SERVICE LINES AND CAP ALL LINES BEFORE PROCEEDING WITH THE WORK.
7. ELECTRICAL, TELEPHONE, CABLE, WATER, FIBER OPTIC, AND/OR GAS LINES NEEDING TO BE REMOVED OR RELOCATED SHALL BE COORDINATED WITH THE AFFECTED UTILITY COMPANY. ADEQUATE TIME SHALL BE PROVIDED FOR RELOCATION AND CLOSE COORDINATION WITH THE UTILITY COMPANY IS NECESSARY TO PROVIDE A SMOOTH TRANSITION IN UTILITY SERVICE. CONTRACTOR SHALL PAY CLOSE ATTENTION TO EXISTING UTILITIES WITHIN ANY ROAD RIGHT-OF-WAY DURING CONSTRUCTION.
8. CONTRACTOR MUST PROTECT THE PUBLIC AT ALL TIMES WITH FENCING, BARRICADES, ENCLOSURES, ETC. (AND OTHER APPROPRIATE BEST MANAGEMENT PRACTICES) AS APPROVED BY THE CONSTRUCTION MANAGER. MAINTENANCE OF TRAFFIC CONTROL SHALL BE COORDINATED IN ACCORDANCE WITH BLOOMINGTON, MINNEAPOLIS COUNTY AND MINN. DOT.
9. CONTINUOUS ACCESS SHALL BE MAINTAINED FOR THE SURROUNDING PROPERTIES AT ALL TIMES DURING DEMOLITION OF THE EXISTING FACILITIES.
10. PRIOR TO DEMOLITION OCCURRING, ALL EROSION CONTROL DEVICES ARE TO BE INSTALLED.
11. CONTRACTOR MAY LIMIT SAWCUT AND PAVEMENT REMOVAL TO ONLY THOSE AREAS WHERE IT IS REQUIRED AS SHOWN ON THESE CONSTRUCTION PLANS BUT IF ANY DAMAGES IS INCURRED ON ANY OF THE SURROUNDING PAVEMENT, ETC. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ITS REMOVAL AND REPAIR.
12. THE CONTRACTOR SHALL COORDINATE WATER MAIN WORK WITH THE FIRE DEPT. AND THE CITY UTILITY DEPARTMENT TO PLAN PROPOSED IMPROVEMENTS AND TO ENSURE ADEQUATE FIRE PROTECTION IS CONTINUITY AVAILABLE TO THE SITE THROUGHOUT THIS SPECIFIC WORK AND THROUGH ALL PHASES OF CONSTRUCTION. CONTRACTOR WILL BE RESPONSIBLE FOR ARRANGING PROVIDING ANY REQUIRED WATER MAIN SHUT OFFS WITH THE CITY OF BLOOMINGTON DURING CONSTRUCTION. ANY COSTS ASSOCIATED WITH WATER MAIN SHUT OFFS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND NO EXTRA COMPENSATION WILL BE PROVIDED.
13. DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN WILL BE REPLACED AT CONTRACTORS EXPENSE.
14. REFER TO SURVEY FOR ALL EXISTING INVERT AND FIN ELEVATIONS.
15. ALL UTILITIES SHOWN ARE EXISTING UTILITIES.
16. A WELL IS ANTICIPATED AND IN THE EVENT ANOTHER WELL IS FOUND, THE CONTRACTOR SHALL CONTACT THE ENGINEER. ALL WELLS SHALL BE SEALED BY A LICENSED WELL CONTRACTOR IN ACCORDANCE WITH ALL MINNESOTA REQUIREMENTS.
17. IN THE EVENT THAT UNKNOWN CONTAINERS OR TANKS ARE ENCOUNTERED, THE CONTRACTOR SHALL CONTACT THE DEC IMMEDIATELY. ALL CONTAINERS SHALL BE DEPOSED OF AT A PERMITTED LANDFILL AND PER THE PROJECT SPECIFICATIONS.

**Bloomington Central  
Station**

**Kimley-Horn**

| App. | No. | Date | Revisions      |
|------|-----|------|----------------|
|      | 1   |      | CITY SUBMITTAL |

DESIGNED BY: N.J.L.  
DRAWN BY: M.R.A.  
CHECKED BY: T.J.L.  
APPROVED:  
EXISTING CONDITIONS AND  
REMOVALS - OVERALL

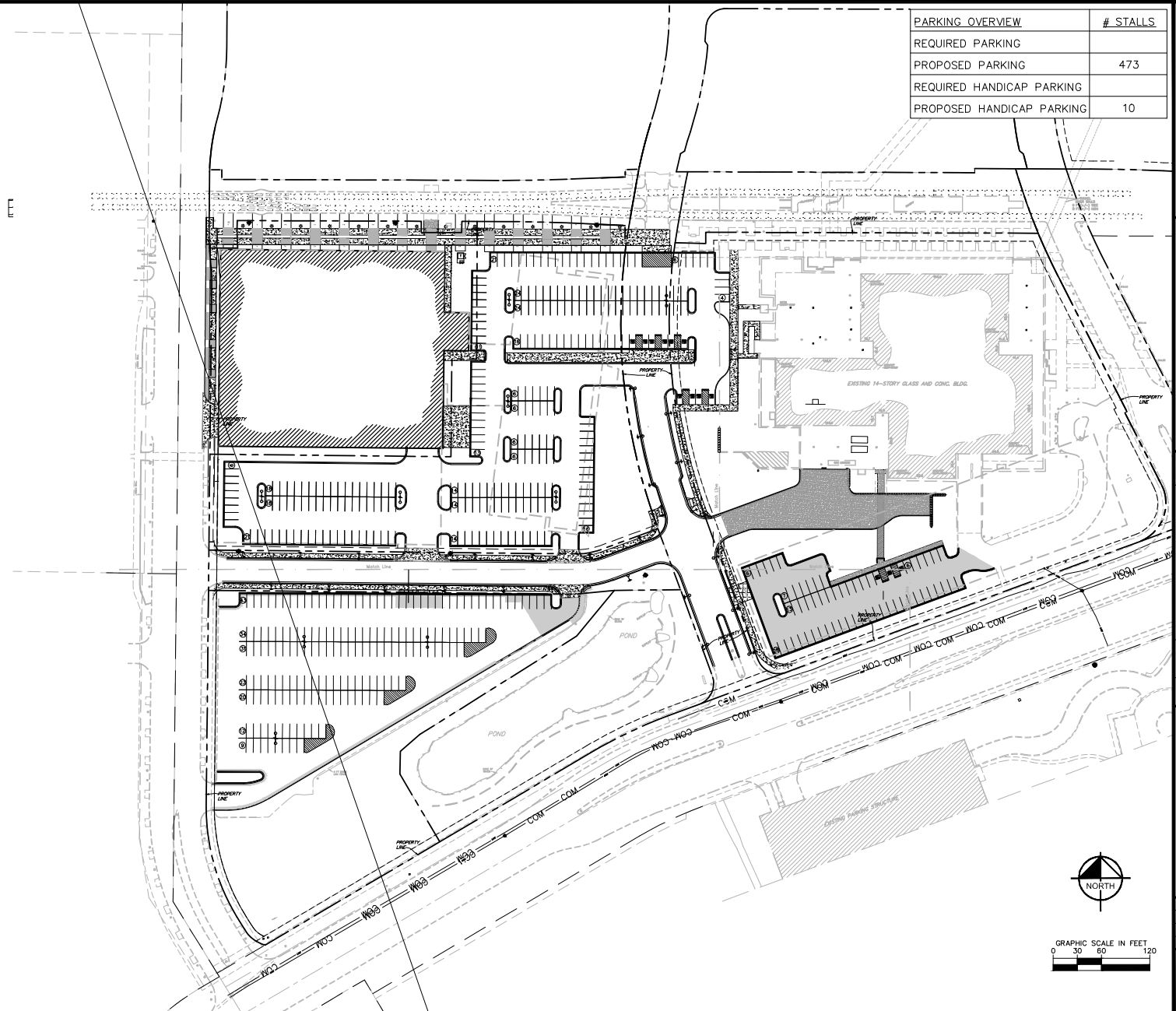
THOMAS J. LINCOLN  
MIN. LIC. NO. 21433  
DATE:

C2.0

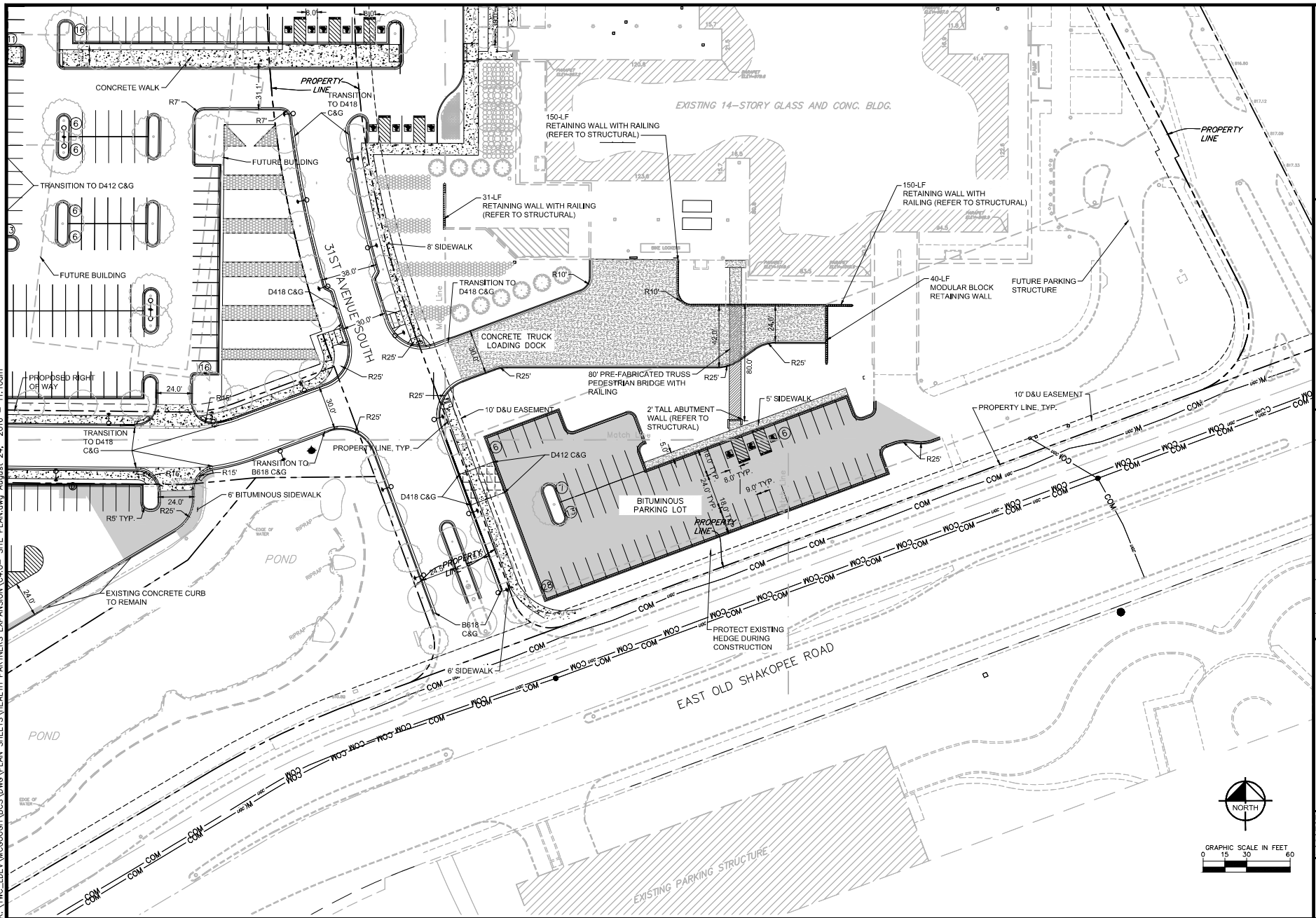
| LEGEND |                             |
|--------|-----------------------------|
|        | PROPERTY LINE               |
|        | PROPOSED FENCE              |
|        | FULL DEPTH SAWCUT           |
|        | SETBACK LINE                |
|        | RETAINING WALL              |
|        | PROPOSED CURB AND GUTTER    |
|        | PROPOSED HEAVY DUTY ASPHALT |
|        | PROPOSED CONCRETE PAVEMENT  |
|        | PROPOSED BRIDGE             |

### SITE PLAN NOTES

1. ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY/COUNTY REGULATIONS AND CODES AND U.S.A. STANDARDS.
2. CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIST. FENCES, TRUCK DOCKS, PROPOSE BUILDING DIMENSIONS AND EXIST BUILDING UTILITY ENTRANCE LOCATIONS.
3. ALL DISTURBED AREAS ARE TO RECEIVE FOUR INCHES OF TOPSOIL, SEED, MULCH AND WATER UNTIL A HEALTHY STAND OF GRASS IS ESTABLISHED.
4. ALL INNER CURBED RADI ARE TO BE 3' AND OUTER CURBED RADI ARE TO BE 10' UNLESS OTHERWISE NOTED. STRIPED RADI ARE TO BE 5'.
5. ALL DIMENSIONS AND RADI ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
6. EXISTING STRUCTURES WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED AS NECESSARY. ALL COST SHALL BE INCLUDED IN BASE BID.
7. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, (UNLESS OTHERWISE NOTED ON PLANS INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS & POLES, ETC. AS REQUIRED). ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES REQUIREMENTS AND PROJECT SITE WORK SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN BASE BID.
8. SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY SUNDEL.
9. Pylon / Monument Signs shall be constructed by others. Signs are shown for GRAPHICAL & INFORMATIONAL PURPOSES ONLY. CONTRACTOR TO VERIFY SIZE, LOCATION AND ANY REQUIRED PERMITS NECESSARY FOR THE CONSTRUCTION OF THE Pylon / Monument SIGN.
10. REFER TO ARCH. PLANS FOR SITE LIGHTING ELECTRICAL PLAN.
11. THERE ARE 0.00 ACRES OF WETLAND IMPACTS.



K:\TWC-LDEV\MCDOUGH\BOS\DWG\PLAN SHEETS\HEALTH PARTNERS EXPANSION C4.0 - SITE PLAN.dwg August 24, 2016 - 11:10am

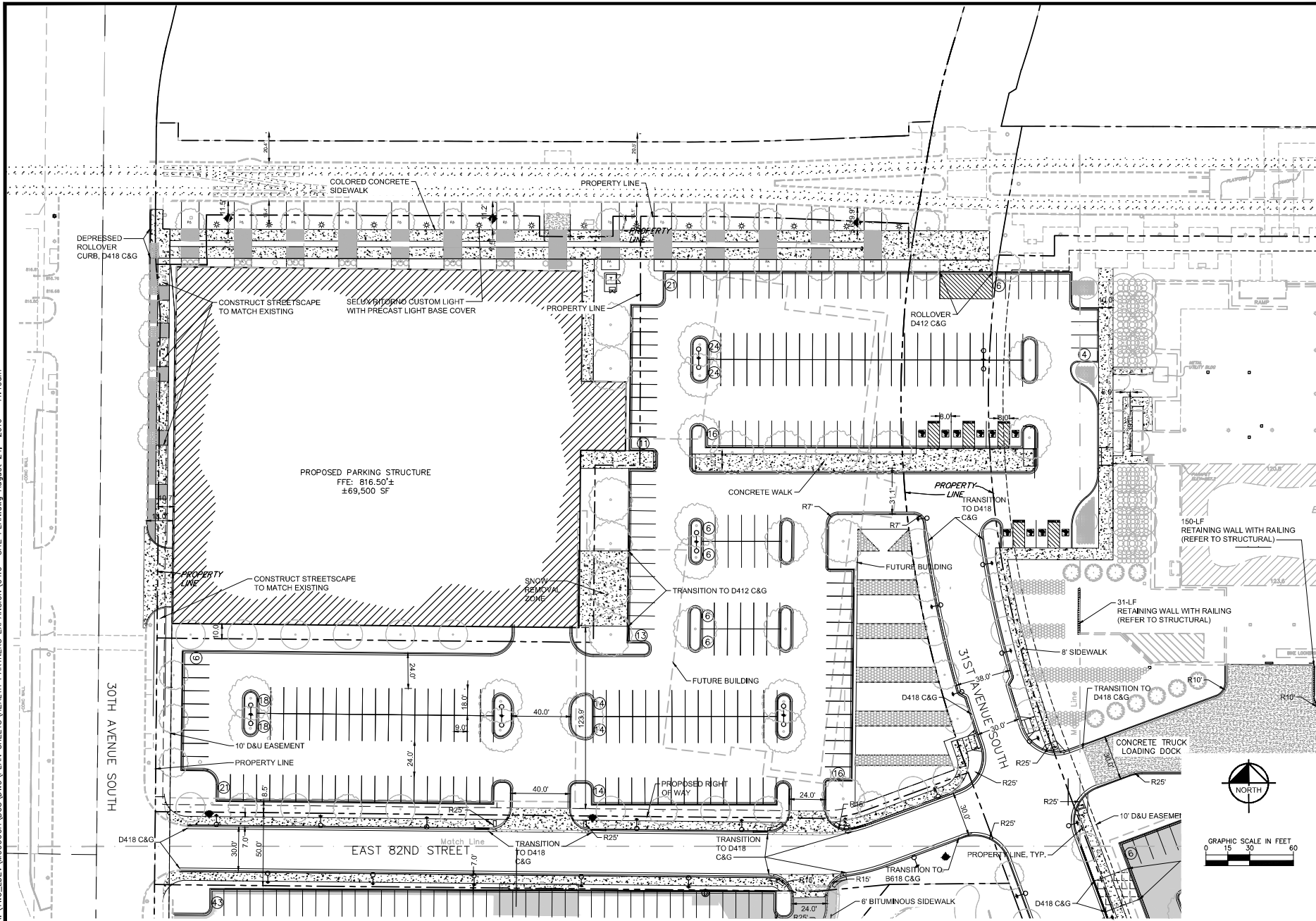


| No. | Date | Revisions | App. |
|-----|------|-----------|------|
|     |      |           |      |
|     |      |           |      |
|     |      |           |      |
|     |      |           |      |

DESIGNED BY: N.J.  
DRAWN BY: M.R.A.  
CHECKED BY: T.J.L.  
APPROVED:  
SITE PLAN - SOUTH

THOMAS J. LINCOLN  
M.N. LIC. NO. 21433  
DATE:  

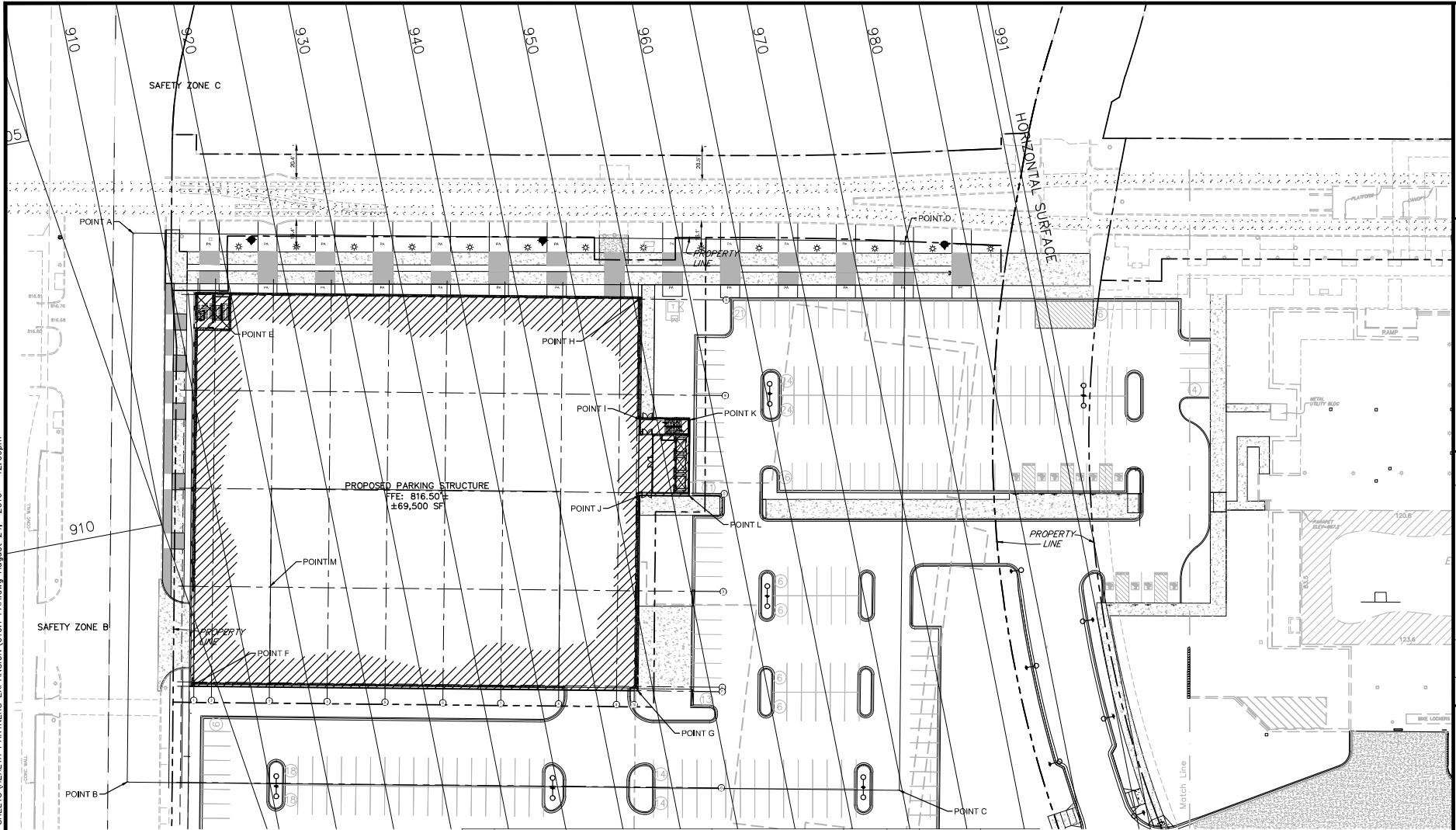
C4.1



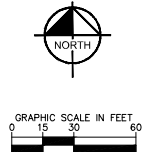




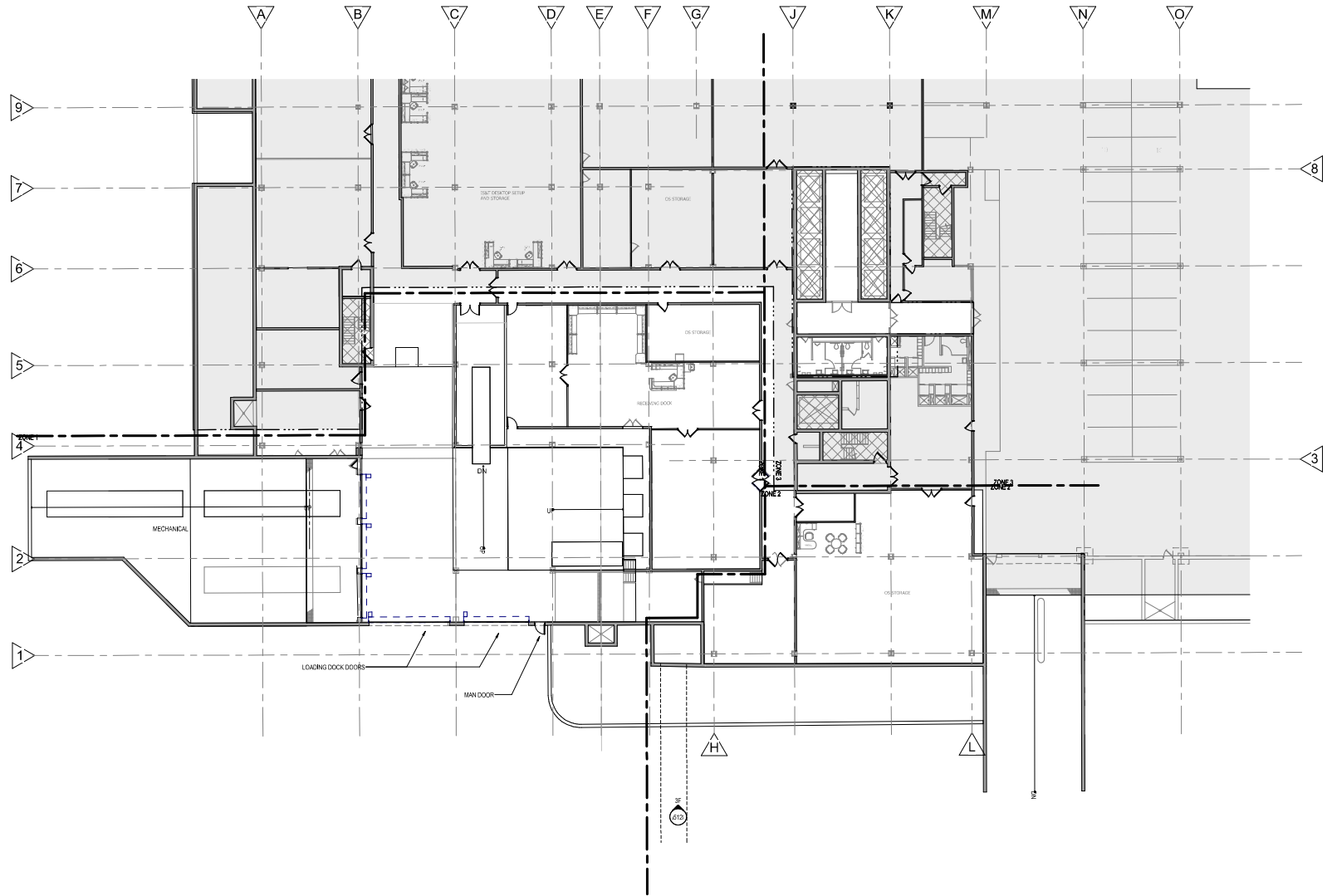
K:\TWC-LDEV\MCDOUGHBGS\DWG\PLAN SHEETS\HEALTH PARTNERS EXPANSION\C10.1 FAA.dwg August 24, 2016 12:09pm



| POINT | DESCRIPTION         | GEODEIC POSITION (NAD 83, MINNESOTA, SOUTH ZONE) |                 | MINNESOTA SOUTH STATE PLANE COORDINATES (feet) |            | GROUND ELEVATION (feet) | OBIC1 HEIGHT (feet) | CONSTRUCTION OBIC1 ELEVATION (feet) | OFFSET FROM RUNWAY 35-17 CENTERLINE (feet) |
|-------|---------------------|--|-----------------|--|------------|-------------------------|---------------------|-------------------------------------|--|
|       |                     | LATITUDE   | LONGITUDE       | NORTHING                                       | EASTING    |                         |                     |                                     |  |
| A     | Work Zone (Crane)   | N44° 53' 22.77"                                  | W93° 13' 47.33" | 1006731.01                                     | 2624383.53 | 806.8                   | 226                 | 1031.8                              | 1091.25                                    |
| B     | Work Zone (Crane)   | N44° 51' 19.42"                                  | W93° 13' 47.39" | 1005392.46                                     | 2624382.58 | 806.8                   | 226                 | 1031.8                              | 1022.23                                    |
| C     | Work Zone (Crane)   | N44° 51' 19.37"                                  | W93° 13' 41.58" | 1005393.30                                     | 2624380.63 | 816.5                   | 226                 | 1042.5                              | 1438.61                                    |
| D     | Work Zone (Crane)   | N44° 51' 22.72"                                  | W93° 13' 01.52" | 1005730.24                                     | 2624800.83 | 816.5                   | 226                 | 1042.5                              | 1528.26                                    |
| E     | Parking Ramp Corner | N44° 51' 22.42"                                  | W93° 13' 46.78" | 1005696.55                                     | 2624423.28 | 816.5                   | 69.5                | 886.0                               | 1121.53                                    |
| F     | Parking Ramp Corner | N44° 51' 20.01"                                  | W93° 13' 46.82" | 1005452.27                                     | 2624422.73 | 816.5                   | 69.5                | 886.0                               | 1074.17                                    |
| G     | Parking Ramp Corner | N44° 51' 19.97"                                  | W93° 13' 42.96" | 1005451.61                                     | 2624701.00 | 816.5                   | 69.5                | 886.0                               | 1347.23                                    |
| H     | Parking Ramp Corner | N44° 51' 23.17"                                  | W93° 13' 42.94" | 1005373.05                                     | 2624799.41 | 816.5                   | 69.5                | 886.0                               | 1370.77                                    |
| I     | Elevator Corner     | N44° 51' 21.54"                                  | W93° 13' 42.92" | 1005600.06                                     | 2624701.48 | 816.5                   | 83.5                | 900.0                               | 1379.26                                    |
| J     | Elevator Corner     | N44° 51' 22.38"                                  | W93° 13' 42.92" | 1005687.90                                     | 2624700.54 | 816.5                   | 83.5                | 900.0                               | 1394.59                                    |
| K     | Elevator Corner     | N44° 51' 21.63"                                  | W93° 13' 42.51" | 1005619.98                                     | 2624797.08 | 816.5                   | 83.5                | 900.0                               | 1410.09                                    |
| L     | Elevator Corner     | N44° 51' 21.17"                                  | W93° 13' 42.51" | 1005572.05                                     | 2624792.01 | 816.5                   | 83.5                | 900.0                               | 1400.94                                    |
| M     | Light Pole          | N44° 51' 30.55"                                  | W93° 13' 46.12" | 1005508.16                                     | 2624772.77 | 816.5                   | 25                  | 911.0                               | 1134.17                                    |



1H FLOOR PLAN - LOWER LEVEL  
A1.1 1/8" = 1'-0"



BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



**McGough**

**BWBR**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

Issued only for the design professional's use. It is not to be used for any other purpose without the written consent of the design professional.

Name: \_\_\_\_\_  
Date: \_\_\_\_\_ Issue Date: \_\_\_\_\_ Reg. No.: \_\_\_\_\_

Issued For: \_\_\_\_\_

Item: \_\_\_\_\_ Date: \_\_\_\_\_  
FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR  
CONSTRUCTION

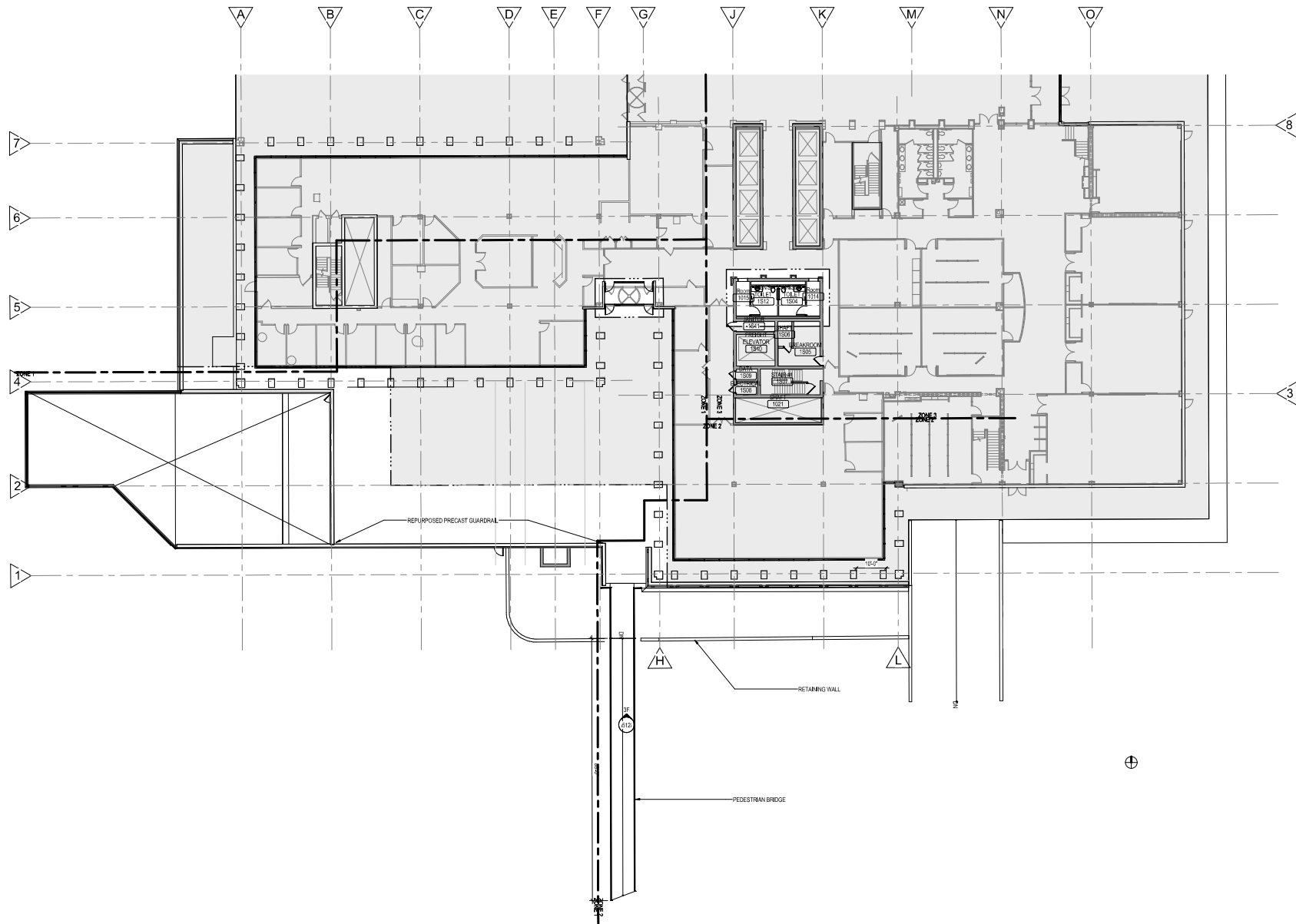
This Sheet may be a  
Reduced Copy  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Comm. No.: \_\_\_\_\_ Drawn: \_\_\_\_\_  
3.2016170.00 DS

Sheet Title:  
**8170 - LOWER LEVEL  
PLAN**

Sheet No.: \_\_\_\_\_

**A1.1**



BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



**McGough**

**B W B R**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwr.com

Consultants

These drawings are the property of the owner and are not to be reproduced or used in any way without the written consent of the owner.

Name: \_\_\_\_\_

Date: \_\_\_\_\_ Issue Date: \_\_\_\_\_ Reg. No.: \_\_\_\_\_

Issued For: \_\_\_\_\_

Item: \_\_\_\_\_ Date: \_\_\_\_\_

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR  
CONSTRUCTION

This Sheet may be a  
Reduced Copy  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Comm. No.: \_\_\_\_\_ Drawn: \_\_\_\_\_

3.2016170.00 DS

Sheet Title: \_\_\_\_\_

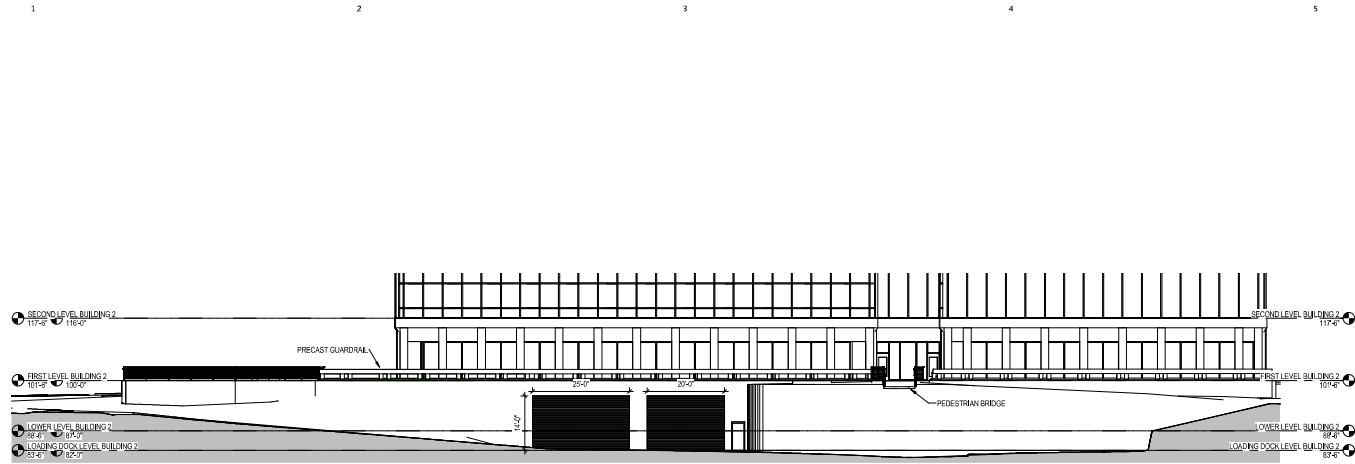
**8170 - LEVEL 1 PLAN**

Sheet No.: \_\_\_\_\_

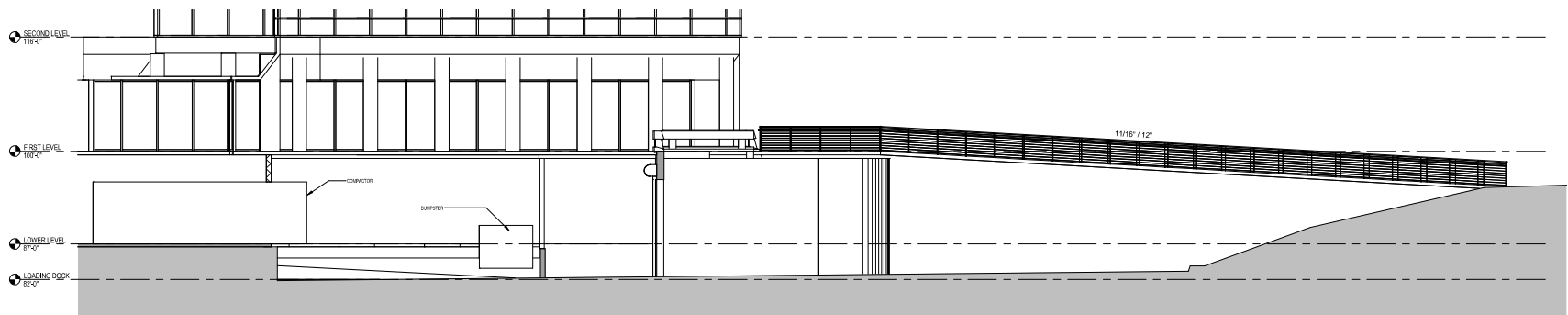
**A1.2**

This document may be an electronic file or may be printed from an electronic file provided to the user. It is the sole responsibility of the user to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWBR.

8/24/2016 9:43:32 AM



1B EXTERIOR ELEVATION 8170 - SOUTH  
A1.3 1/8" = 1'-0"



1H BUILDING SECTION AT 8170 PEDESTRIAN BRIDGE  
A1.3 1/8" = 1'-0"

BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



McGough

BWBR

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

These drawings are for informational purposes only and are not to be used for construction or other purposes without the express written consent of the Engineer.

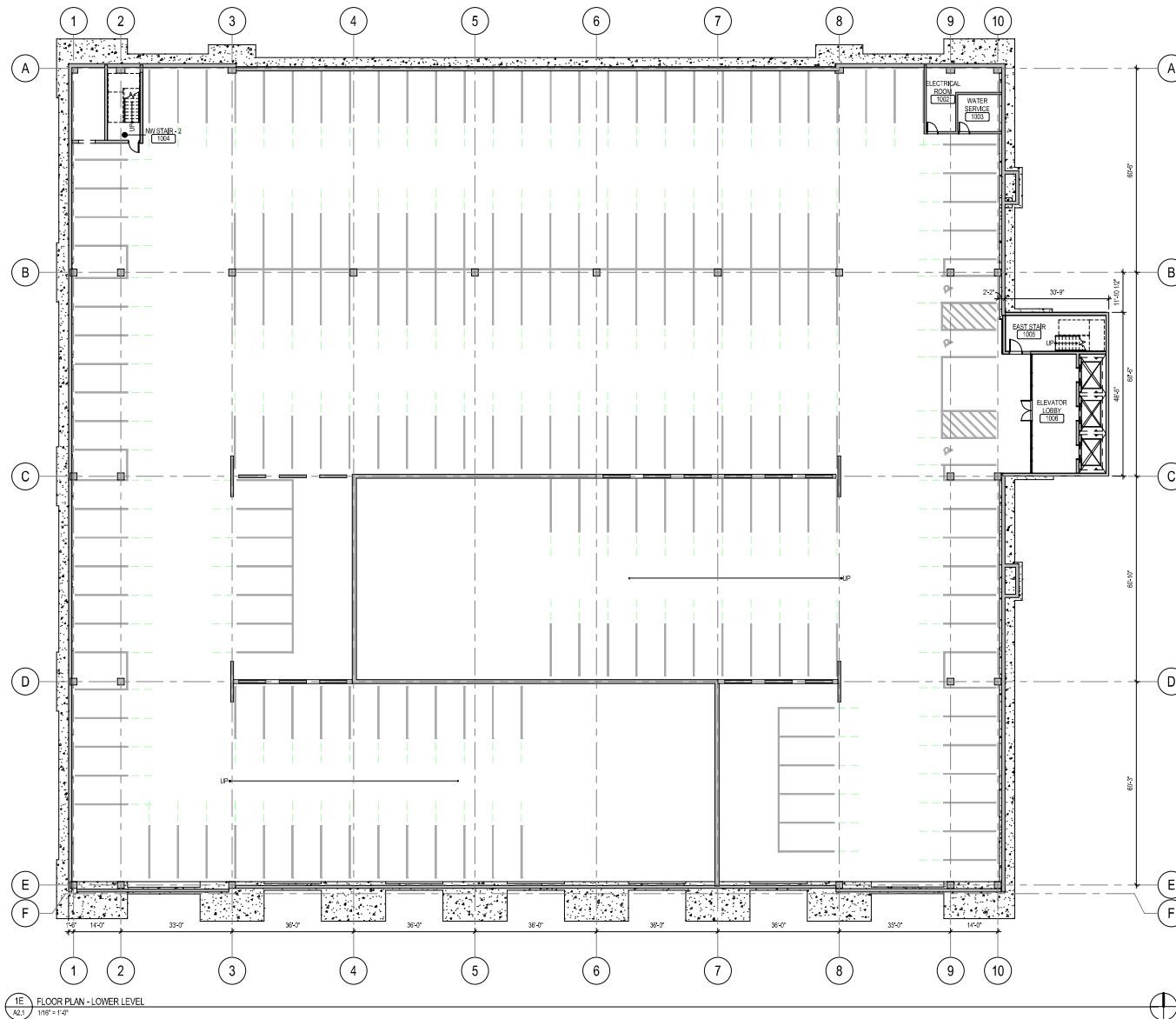
Name:

Date Issue Date Reg. No.

Issued For:

Item Date

8/24/2016 9:43:32 AM This document may be an electronic file or may be printed from an electronic file provided to the user. It is the sole responsibility of the user to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWBR.



BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



**McGough**

**BWBR**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

Issued only for the specific project and for the specific use only.  
Any other use is unauthorized and may result in legal action.  
Consultants are responsible for the accuracy of the information.

Name: \_\_\_\_\_

Date: \_\_\_\_\_ Issue Date: \_\_\_\_\_ Reg. No.: \_\_\_\_\_

Issued For: \_\_\_\_\_

Item: \_\_\_\_\_ Date: \_\_\_\_\_

FINAL DEVELOPMENT PLANS - 08/24/2016

**NOT FOR  
CONSTRUCTION**

This Sheet may be a  
Reduced Copy.  
The bar above is 1" long on a full size sheet.  
Drawing scales apply to full size sheets.

Contract No.: \_\_\_\_\_ Drawn: \_\_\_\_\_

3.2016170.00 SD

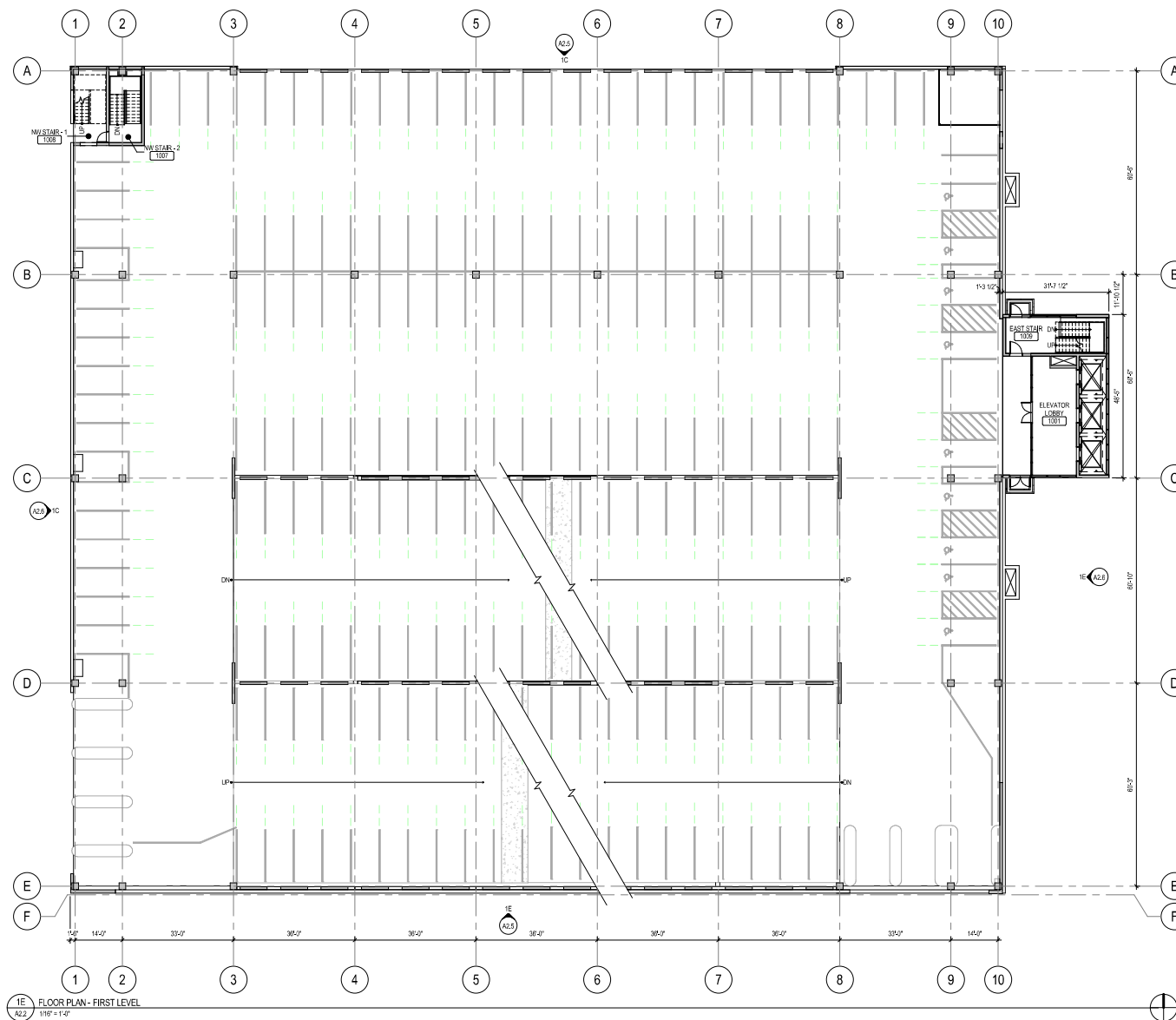
Sheet Title:

**RAMP - LOWER LEVEL  
PLAN**

Sheet No.: \_\_\_\_\_

**A2.1**

Copyright BWBR

BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS

**McGOUGH**

|   |   |   |   |
|---|---|---|---|
| B | W | B | R |
|---|---|---|---|

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
hwyb.com

**B Consultants**

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed professional engineer in the State of California.

Name \_\_\_\_\_

Date Issue Date

Reg. No.


Issued For

Item

Date \_\_\_\_\_

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR CONSTRUCTION

 This Sheet may be a Reduced Copy.

The bar above is 1" long on a Full Size Sheet.  
Drawing Scales apply to Full Size Sheets.

|   |              |
|---|--------------|
|   | Comm. No.    |
| E | 3.2016170.00 |

Drawn

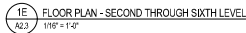
Client Title

**Author**

**RAMP - FIRST LEVEL  
PLAN**

Sheet No. \_\_\_\_\_

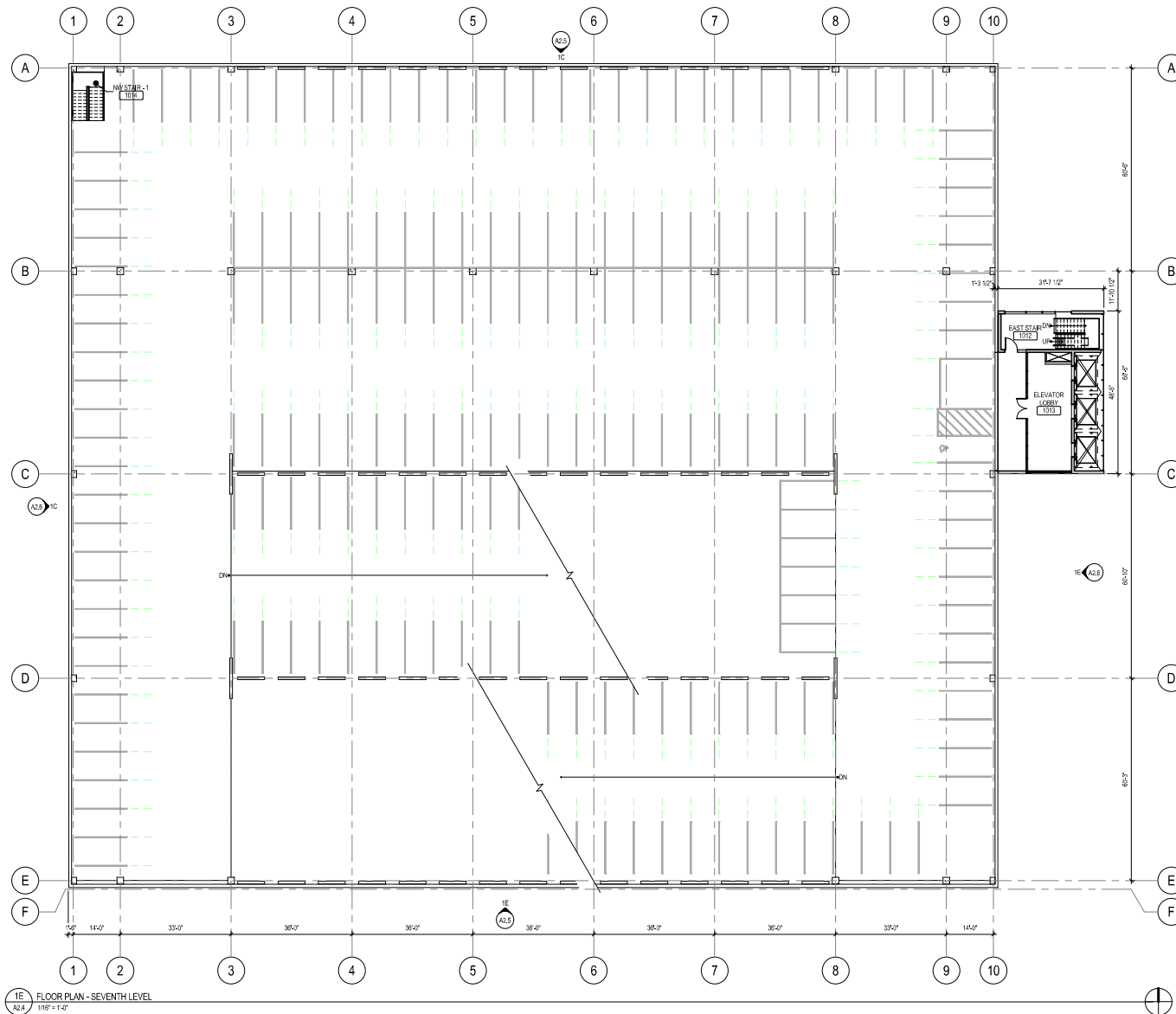
## A2.2





This document may be an electronic file or may be printed from an electronic file provided to the user. It is the sole responsibility of the user to ensure that the content and quality of the paper documents on file at BWRB.

8/24/2016 9:43:33 AM



BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



**McGough**

**B W B R**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

These drawings are the property of the consultant and are not to be reproduced or used in any manner without the written consent of the consultant.

Name: \_\_\_\_\_

Date: \_\_\_\_\_ Issue Date: \_\_\_\_\_ Reg. No.: \_\_\_\_\_

Issued For: \_\_\_\_\_

Item: \_\_\_\_\_ Date: \_\_\_\_\_

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR  
CONSTRUCTION

This Sheet may be a  
Reduced Copy  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Contract No.: \_\_\_\_\_ Drawn: \_\_\_\_\_

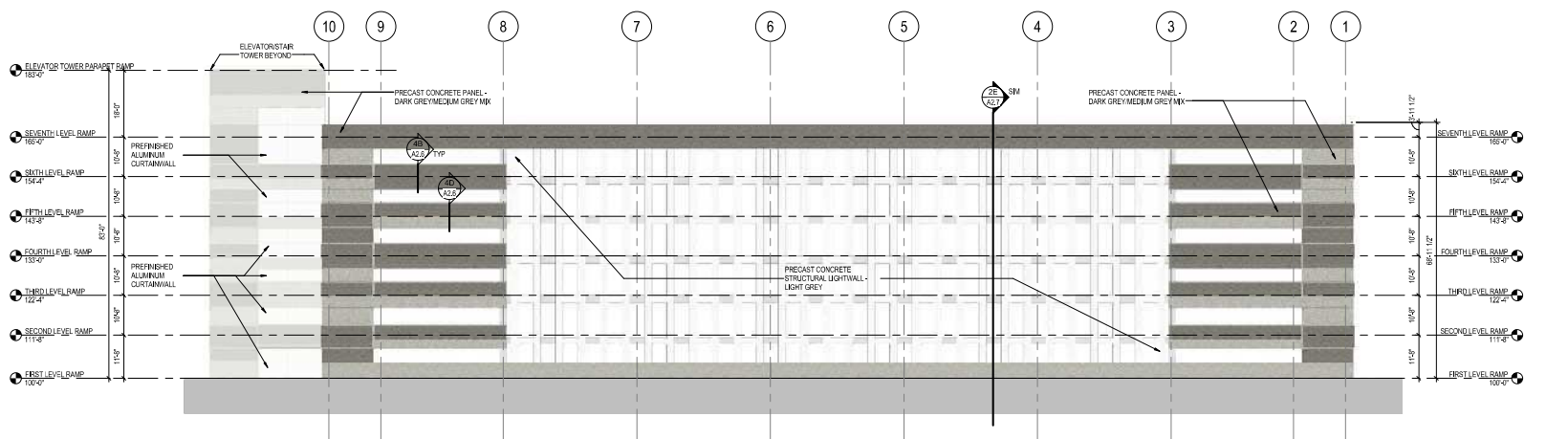
3.2016170.00 Author: \_\_\_\_\_

Sheet Title:  
**RAMP - SEVENTH LEVEL  
PLAN**

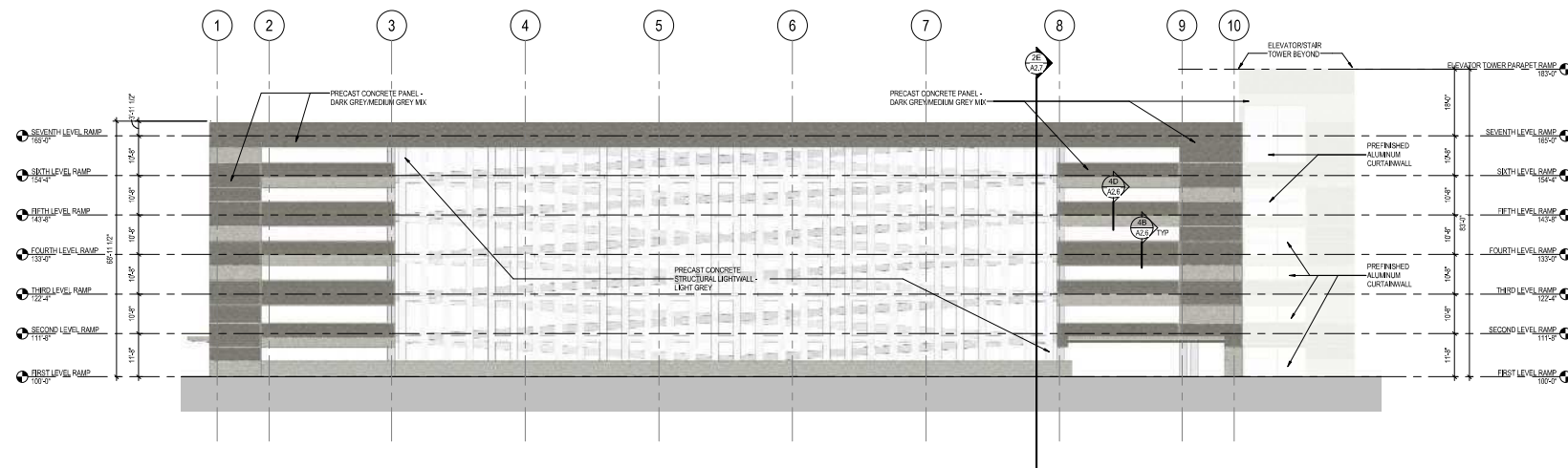
Sheet No.: \_\_\_\_\_

**A2.4**

Copyright BWRB



1C PRESENTATION ELEVATION - NORTH  
A2.5 1/16" = 1'-0"



1E PRESENTATION ELEVATION - SOUTH  
A2.5 1/16" = 1'-0"

BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS

**McGough**

|   |   |   |   |
|---|---|---|---|
| B | W | B | R |
|---|---|---|---|

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
hwyb.com

**B Consultants**

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am, and I remain, a citizen of the State of Minnesota.

Name \_\_\_\_\_

| Date | Issue Date | Reg. No. |
|------|------------|----------|
|------|------------|----------|

Issued For

| Item                    | Date       |
|-------------------------|------------|
| ETNAI DEVELOPMENT PLANS | 08/24/2013 |

|                                 |       |
|---------------------------------|-------|
| FINANCIAL PHYSICIAN CONSULT FEE | 20.00 |
|                                 |       |
|                                 |       |

---

FOR OFFICIAL USE ONLY

NO  
TRU

\_\_\_\_\_

[illegible]

---

---

[illegible]

Reduced Copy.

Drawing Scales apply to Full Size Sheets.

\_\_\_\_\_

|           |       |
|-----------|-------|
| Comm. No. | Drawn |
|-----------|-------|

|              |        |
|--------------|--------|
| 3.2016170.00 | Author |
|--------------|--------|

## RAMP - EXTERIOR

## ELEVATIONS

\_\_\_\_\_

\_\_\_\_\_

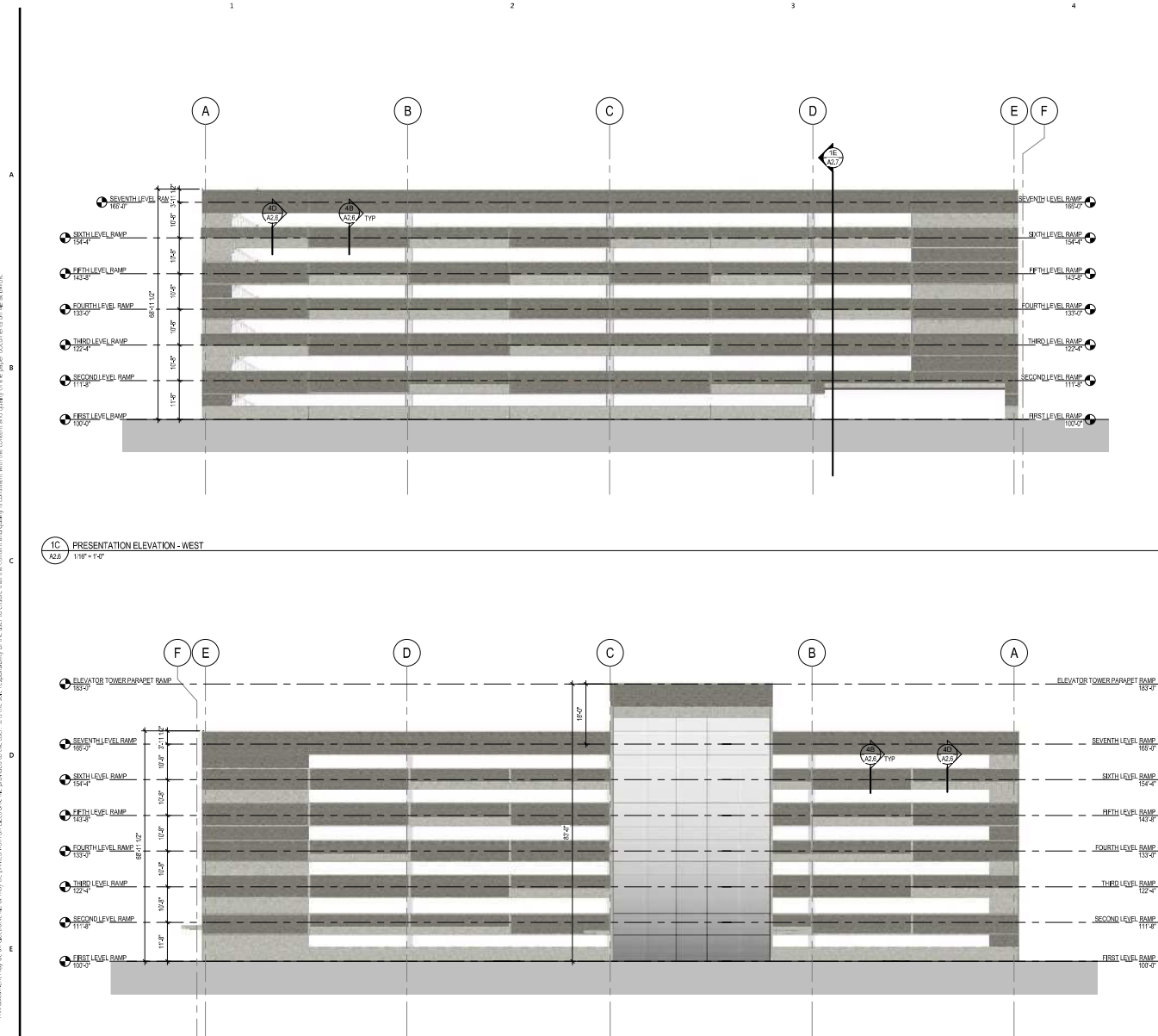
Sheet No. \_\_\_\_\_

A25

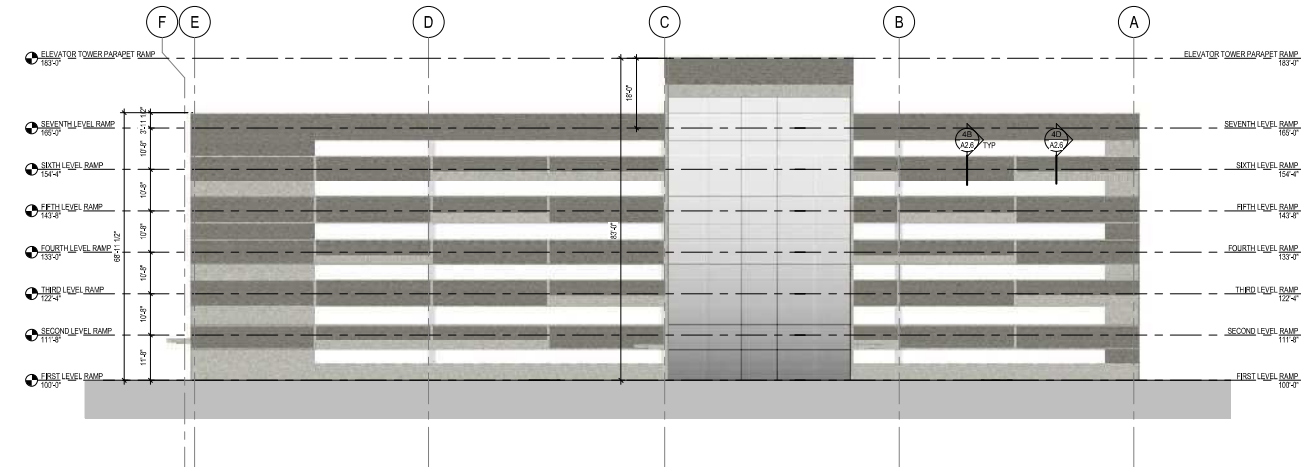
## A2.5

1. *Journal of Management Studies*, 1990, 27, 1.

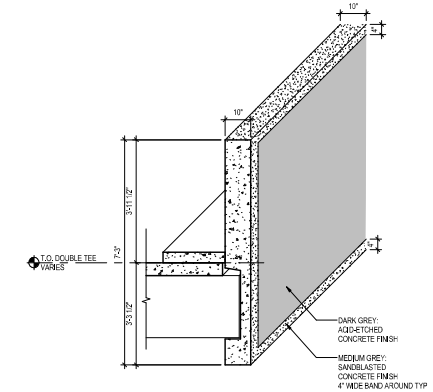
\_\_\_\_\_



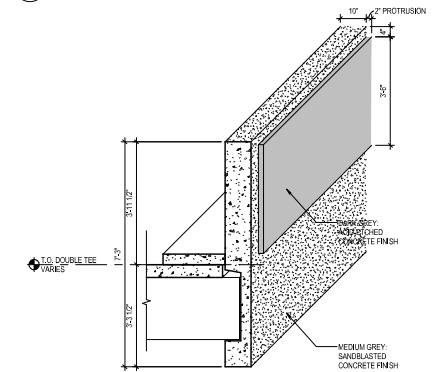
10 PRESENTATION ELEVATION - WEST  
A2.6 1/8" = 1'-0"



11 PRESENTATION ELEVATION - EAST  
A2.6 1/8" = 1'-0"



4B TYPICAL FLAT PRECAST CONCRETE SPANDELEL PANEL  
A2.6 1/2" = 15'



4D PRECAST CONCRETE SPANDELEL PANEL WITH 2" PROTRUSION  
A2.6 1/2" = 15'

# BCS/HP PARKING RAMP AND 8170 IMPROVEMENTS



**McGough**

**BWBR**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consents

Issued only for the design and construction of the project.  
No other use or reproduction is permitted without the written consent of the design professional.

Name: \_\_\_\_\_  
Date: \_\_\_\_\_ Issue Date: \_\_\_\_\_ Reg. No.: \_\_\_\_\_

Issued For: \_\_\_\_\_

Item: \_\_\_\_\_ Date: \_\_\_\_\_  
FINAL DEVELOPMENT PLANS: 08/24/2016

NOT FOR CONSTRUCTION

This Sheet may be a  
Reduced Copy  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

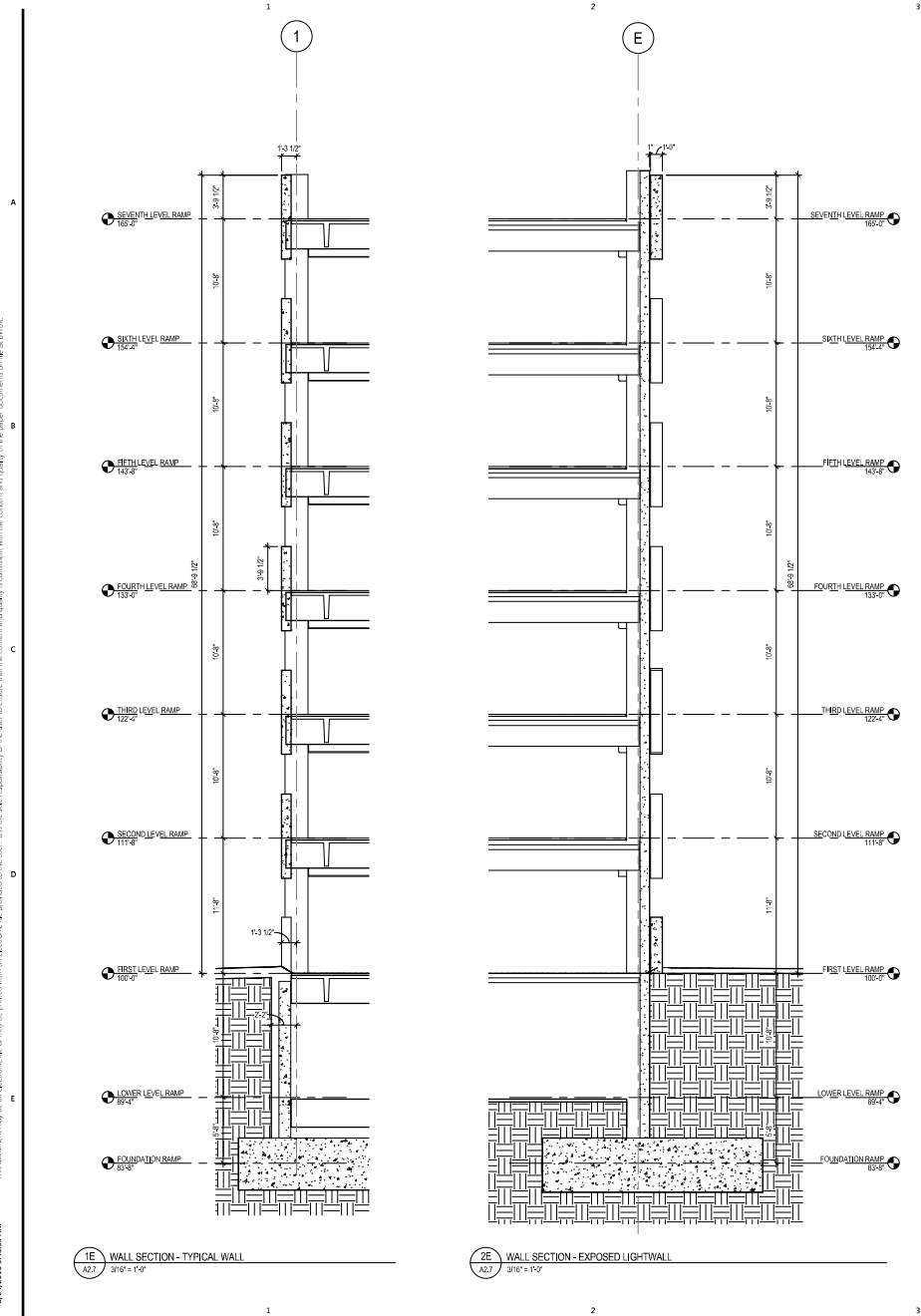
Contm. No.: \_\_\_\_\_ Drawn: \_\_\_\_\_  
3.2016170.00 Author: \_\_\_\_\_

Sheet Title:  
**RAMP - EXTERIOR  
ELEVATIONS**

Sheet No.: \_\_\_\_\_

**A2.6**

Copyright BWBR



BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



McGough

BWBR

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

These drawings are for informational purposes only and are not to be used for construction purposes without the approval of the design professional.

Name:

Date Issue Date Reg. No.

Issued For:

Item Date

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR CONSTRUCTION

This Sheet may be a Reduced Copy  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Comm. No. Drawn

3.2016170.00 Author

Sheet Title

RAMP - WALL SECTIONS

Sheet No.

A2.7

This document may be an electronic file or may be printed from an electronic file provided to the user. It is the sole responsibility of the user to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWBR.

8/24/2016 9:43:35 AM



PERSPECTIVE - SOUTHEAST APPROACH

BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



**McGough**

**B W B R**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

These drawings are the property of the client and are not to be reproduced or used in any way without the written consent of the client.

Name:

Date: Issue Date: Reg. No.:

Issued For:

Item: Date:

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR  
CONSTRUCTION

This Sheet may be a  
Reduced Copy.  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Comm. No. Drawn:

3.2016170.00 Author:

Sheet Title:

**RAMP - EXTERIOR  
IMAGES**

Sheet No.:

**A2.8**

Copyright BWBR



This document may be an electronic file or may be printed from an electronic file provided to the user. It is the sole responsibility of the user to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWBR.

8/24/2016 9:43:35 AM



PERSPECTIVE - SOUTHWEST APPROACH

BCS/HP PARKING  
RAMP AND 8170  
IMPROVEMENTS



**McGough**

**B W B R**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

Intend only for the use of the user. It is the sole responsibility of the user to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWBR.

Name

Date Issue Date Reg. No.

Issued For

Item Date

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR  
CONSTRUCTION

This Sheet may be a  
Reduced Copy.  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Comm. No. Drawn

3.2016170.00 Author

Sheet Title

**RAMP - EXTERIOR  
IMAGES**

Sheet No.

**A2.9**

Copyright BWBR

This document may be an electronic file or may be printed from an electronic file provided to the user. It is the sole responsibility of the user to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWBR.

8/24/2016 9:43:36 AM



PERSPECTIVE - NORTHWEST APPROACH

# BCS/HP PARKING RAMP AND 8170 IMPROVEMENTS



**McGough**

**BWBR**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

These drawings are for informational purposes only.  
They are not to be used for construction or other purposes.  
Consultants are not responsible for the design of the project.

Name:

Date Issue Date

Reg. No.

Issued For:

Item:

Date:

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR  
CONSTRUCTION

This Sheet may be a  
Reduced Copy.  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Comm. No.

Drawn

3.2016170.00

Author

Sheet Title:

**RAMP - EXTERIOR  
IMAGES**

Sheet No.

**A2.10**

Copyright BWBR

A  
B  
C  
D  
E

1

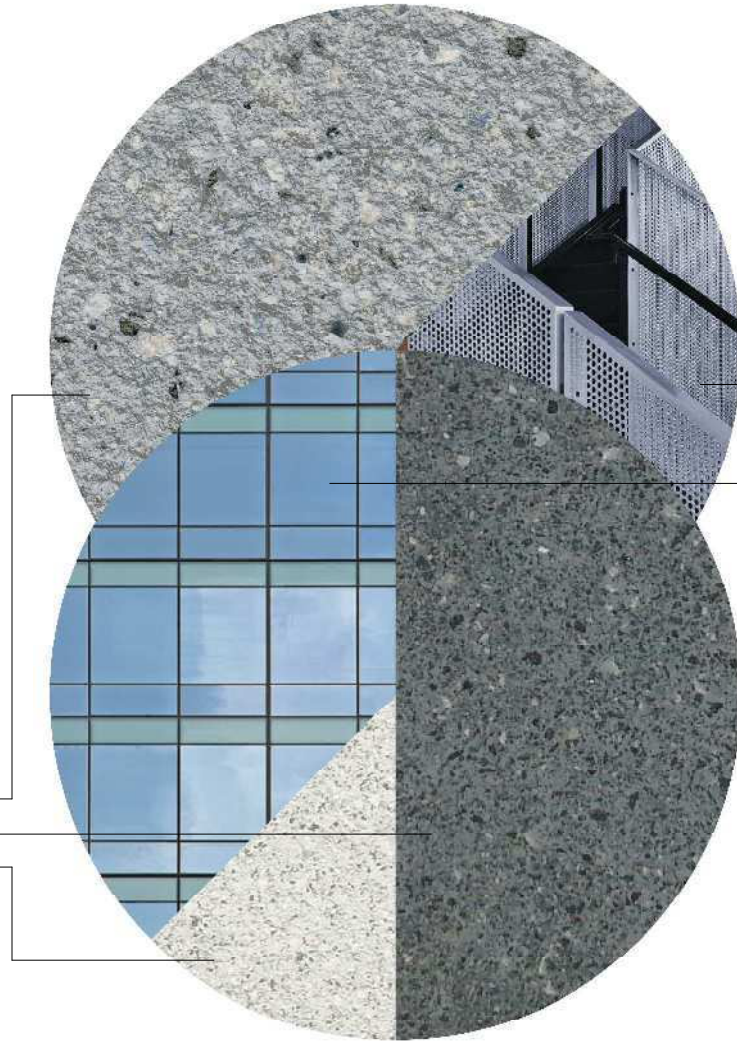
2

3

4

5

MEDIUM GRAY SANDBLASTED PRECAST CONCRETE PANEL  
DARK GRAY ACID-ETCHED PRECAST CONCRETE PANEL  
LIGHT GRAY PRECAST CONCRETE STRUCTURAL MEMBER



PERFORATED METAL PANEL RAILING  
PREFINISHED ALUMINUM CURTAINWALL

# BCS/HP PARKING RAMP AND 8170 IMPROVEMENTS



**McGough**

**B W B R**

380 St. Peter Street, Ste. 600  
Saint Paul, MN 55102  
651.222.3701  
bwbr.com

Consultants

Issued only for the design professional's use. It is the user's responsibility to ensure that the content and quality is consistent with the content and quality of the paper documents on file at BWRB.

Name:

Date Issue Date Reg. No.

Issued For:

Item Date

FINAL DEVELOPMENT PLANS 08/24/2016

NOT FOR  
CONSTRUCTION

This Sheet may be a  
Reduced Copy  
The bar above is 1" long on a Full Size Sheet.  
Drawing Scale apply to Full Size Sheets.

Comm. No. Drawn

3.2016170.00 Author

Sheet Title:

**RAMP - VIRTUAL  
MATERIAL BOARD**

Sheet No.:

**A2.11**

Copyright BWRB

1

2

3

4

5









BCS HEALTH  
PARTNERS

[illegible]

**LANDSCAPE ARCHITECTURE**  
**MASTER PLANNING**  
**ENVIRONMENTAL DESIGN**  
MINNEAPOLIS MN 55401  
SUITE 200  
115 WASHINGTON AVE N  
TEL 612.632.5509  
[www.zbjs.com](http://www.zbjs.com)

Name \_\_\_\_\_

Date \_\_\_\_\_

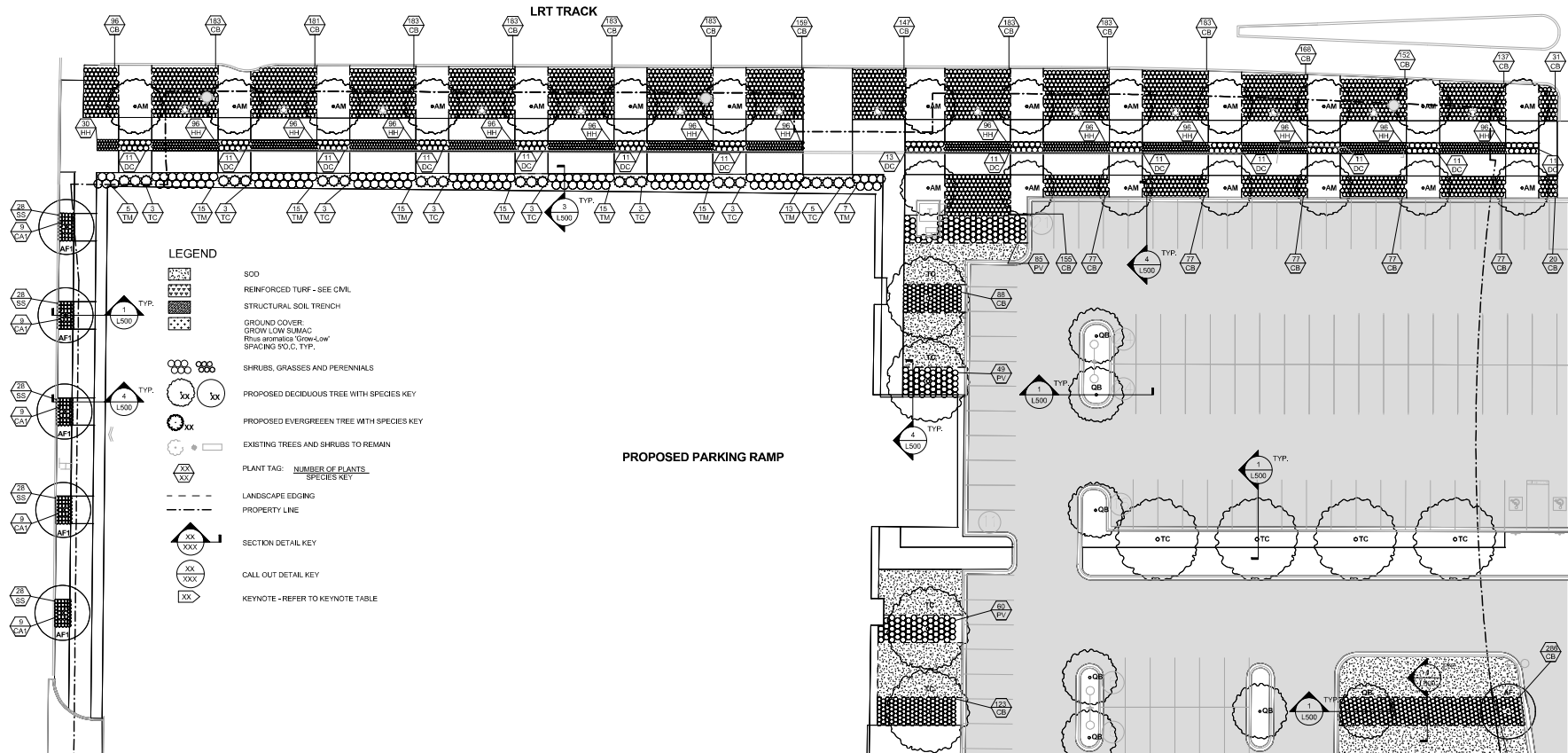
Registration Number \_\_\_\_\_

Signature \_\_\_\_\_

# PLANTING PLAN AREA A

|                 |             |
|-----------------|-------------|
| Date:           | 08/24/16    |
| Scale:          | 1" = 20'-0" |
| Project Number: | 2635.005    |
| Drawn:          | HZH/RH      |

© Copyright oslund, and assoc.









[illegible]

LANDSCAPE ARCHITECTURE  
MASTER PLANNING  
ENVIRONMENTAL DESIGN  
MINNEAPOLIS MN 55401  
SUITE 200  
115 WASHINGTON AVE N  
TEL 612.532.5509  
[www.orellu.com](http://www.orellu.com)

Name \_\_\_\_\_

---

---

\_\_\_\_\_

Signature \_\_\_\_\_

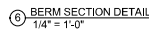
ISSUED FOR  
CITY SUBMITTAL

Draw: 06/24

[illegible]

---

© Copyright 2012 and assoc.











# Development Review Committee

## Approved Minutes

Development Application, #PL201600146

Mtg Date: September 13, 2016

McLeod Conference Room

Bloomington Civic Plaza

1800 West Old Shakopee Road

### Staff Present:

Kris Kaiser (Fire Prev) 952-563-8968

Duke Johnson (Bldg & Insp) 952-563-8959

Bruce Bunker (Eng.) 952-563-4546

Eric Wharton (Utilities) 952-563-4579

Erik Solie (Env. Health) 952-563-8978

Mike Thissen (Env. Health) 952-563-8981

Nick Johnson (Planning) 952-563-8925

Vicky Soukaseum (Eng.) 952-563-4627

### Project Information:

|                     |  |
|---------------------|--|
| Project             | Health Partners Parking Structure  |
| Site Address        | 8170 31ST AVE S, BLOOMINGTON, MN 55425   |
| Plat Name           | BLOOMINGTON CENTRAL STATION 2ND ADDN;  |
| Project Description | Type II Preliminary and Final Plat to plat 1 lot and three outlots, Major Revision to Final Development Plan for a 7-story parking structure and surface parking improvements, and an Airport Zoning Permit. |
| Application Type    | Final Development Plan<br>Preliminary Plat - Type II<br>Final Plat - Type II   |
| Staff Contact       | Nick Johnson   |
| Applicant Contact   | Bloomington Central Station LLC c/o McGough Development<br>mfabel@mcgough (651) 248-3024   |
| PC                  | 09/29/2016   |
| CC (tentative)      | 10/24/2016   |

### Guests Present:

| Name           | Email  |
|----------------|--|
| Mark Fabel     | <a href="mailto:mfabel@mcgough.com">mfabel@mcgough.com</a>                     |
| Thomas Lincoln | <a href="mailto:tom.lincoln@kimley-horn.com">tom.lincoln@kimley-horn.com</a>   |
| Eric West      | <a href="mailto:ewest@bwbr.com">ewest@bwbr.com</a>                             |
| Dan Elenbaas   | <a href="mailto:dan.elenbaas@kimley-horn.com">dan.elenbaas@kimley-horn.com</a> |

### Discussion/Comments:

- Nick Johnson (Planning):
  - Type II Preliminary and Final Plat to Bloomington Central Station, Second Addition, to divide one outlot into one lot and three outlots, two outlots to be used as temporary surface parking, one is on the private right of way on 82<sup>nd</sup> Street.
  - Loading dock and surface parking modifications. Pedestrian entrance to the southwest plaza.
  - Major Revision to Final Development Plans for an 8-level parking structure; 1,666 parking stalls and option to expand west to total 2700 parking stalls.
  - In addition, applicant is requesting an Airport Zoning permit.

- Kent Smith (Assessing):
  - Parking deck is not increasing employee count as of right now, so no park dedication is needed at this time.
- Erik Solie (Environmental Health):
  - No Comment.
- Duke Johnson (Building and Inspection):
  - Replatting has removed issues with property line issues. No additional comments.
  - When ready for Preliminary Plan, contact Building and Inspection to review application process.
    - Fabel asked about future platting on Phase II.
    - Lincoln added it is 35,000 sq. ft. of replatting area as Lot 1, Block 1,
    - D. Johnson replied there needs to be legal ramifications that distinguishes how the lots and plats will be divided.
    - Lincoln asked about the plan review process, with C/D's in October and looking for permitting in October-November.
    - D. Johnson said as soon as they are ready, we can have a meeting and set up permitting. Even with 80% plans, it should be okay.
- Kris Kaiser (Fire Prevention):
  - Work with Laura McCarthy on fire access on the north side.
  - Regarding access when coming on 31<sup>st</sup>, what type of surface material would that be?
    - Lincoln answered that he sent Jen Desrude plans with material listings, and waiting for approval.
    - N. Johnson hasn't had any response in this transmittal, and stated he will follow up with Desrude to clarify.
  - Is this open parking structure?
    - West answered that the lower level is mechanically ventilated and sprinklered and the rest is an open parking ramp.
  - D. Johnson asked if the stairwells are heated.
    - West answered one is tempered and two are not.
- Bruce Bunker (Engineering) provided the Public Works comments and noted the following:
  - Any additional questions, please contact Jen Desrude, 952-563-4862 or [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov)
  - Set up a meeting with Bryan Gruidl, Senior Water Resource Manager, 952-563-4557 or [bgruidl@BloomingtonMN.gov](mailto:bgruidl@BloomingtonMN.gov) to discuss the Stormwater Management Plan.
- Eric Wharton (Utilities):
  - For more information and clarification, please contact Tim Kampa, 952-563-8776 or [tkampa@BloomingtonMN.gov](mailto:tkampa@BloomingtonMN.gov).
  - Move landscaping, pond, retaining wall, or other structure out of right-of-way or easement. If it is planned to be moved, there needs to be an encroachment agreement.
    - For more information and clarification, please contact Tim Kampa, 952-563-8776 or [tkampa@BloomingtonMN.gov](mailto:tkampa@BloomingtonMN.gov).
    - Fabel noted that the only street that is against public area is 30<sup>th</sup>. With that, they will be disturbing the area, but not changing anything, it has been agreed that all will return to its original state.

- West added the only area that he can think of that may possibly be encroached is where the entrance sign sits.
  - Install hydrants to provide fire protection for entire building. Each hydrant covers 150-foot radius. There are not enough hydrants at the Health Partners building. And another hydrant should be added on the SE side of the proposed parking ramp. Can Bloomington Fire get to the proposed hydrants on the north side of the ramp? There must be a hydrant located within 50 feet of all building fire dept. connection points.
  - Watermains and Hydrants can be no closer than 20 feet from LRT tracks – To protect the City water system from stray current corrosion.
  - All levels of the parking ramp that have sidewalls more than 50% open must drain to the storm sewer.
    - Elenbaas responded the roof is drained to the stormsewer
    - Wharton added that the lower levels go into the sanitary.
  - Suggests installing the future sewer crossing under the LRT tracks (at 31<sup>st</sup> ST) now to avoid having to disrupt the areas south of the tracks in the future.
  - Check and verify City's water and sewer GIS records against the plans as some valves and pipe sizes are not shown correctly. Please see redlines on plans.
  - Sewer and water are too close on the west side of the parking ramp. Constructability and separation issues for the sanitary sewer and water on the west and north side.
    - Elenbaas asked about the sanitary of the northwest corner of the parking structure has a .4% slope and it's a tight fit right now with the floor drains. Requesting record drawing of the area.
- Nick Johnson (Planning):
  - If metals are to be used as a primary for the elevator structure, it would have to be approved.
    - West answered that it would be glass and pre-cast.
  - Insufficient lighting levels in some areas of the parking lot. Areas would be called out in the redlines.
  - Some landscaping concerns, they are called on landscaping plans.
  - The approved Preliminary Development Plan envisioned a mid-block pedestrian crossing of Blue Line LRT right-of-way. Would this cross be constructed with the proposed phase of construction or a future phase of construction?
    - Fabel answered, while completing south side, it will connect to it. The north side is not connected. With the nature of completing the south side, the north will have to be completed. Will need to coordinate with the right parties and look into it with depth.
  - Fabel asked about the parking lot elevator and stair-with materials, what are the minimum requirements regarding look, design, code, etc.?
    - West added we've seen glass on other buildings nearby, we wanted to know what the regulations were to address heat load issues, cost issues, and other variables.
    - N. Johnson answered some architectural performance standards in the HXR, which has specific language requirements for parking structures, not necessarily material requirements. Needs further review.
    - D. Johnson added it might be more of the State's jurisdiction to have requirements and inspectors come to approve the structure.



## Comment Summary

PL201600146

**Application #:** PL201600146

**Address:** 8170 31ST AVE S, BLOOMINGTON, MN 55425.

**Request:** **Type II Preliminary and Final Plat to divide one outlot into one lot and three outlots; Major Revision to Final Development Plans for a new 8-level parking structure plus loading dock and surface parking modifications. Airport Zoning Permit.**

**Meeting:** Post Application DRC - September 13, 2016  
Planning Commission - September 29, 2016  
City Council (tentative) - October 24, 2016

---

**Planning Review Contact:** Nick Johnson at nmjohnson@BloomingtonMN.gov, (952) 563-8925

- 1) All primary (greater than 15%) metal building materials proposed must comply with the City's metals policy related to durability.
- 2) Parking lot and exterior security lighting must meet Section 21.301.07. A minimum of 1.5 foot-candles is required for all surface parking (which may be reduced to 0.75 foot-candles for the outer perimeter 25 feet of the parking lot). A minimum of 3.0 foot-candles is required for all structure parking (which may be reduced to 1.5 foot-candles for the outer perimeter 25 feet of the parking ramp).
- 3) Trees planted within the 30th Avenue public right-of-way are not eligible to be counted towards landscaping requirements for the subject property per Section 19.52(c)(5) of the City Code. As a result, the amount of landscape material provided is deficient by 6 trees.
- 4) Perimeter parking lot screening (3 to 4 feet in height) required per Sec. 19.52 of City Code.
- 5) 7-foot wide sidewalk required when perpendicular to 90 degree parking.
- 6) Any future signage must be consistent with the Class VII Sign District (Sec. 19.115.01) and the approved Uniform Sign Design for Bloomington Central Station.
- 7) The approved Preliminary Development Plan envisioned a mid-block pedestrian crossing of the Blue Line LRT right-of-way. Would this crossing be constructed with the proposed phase of construction or a future phase of construction?
- 8) An FAA 7460 analysis and crane variance provided by MAC MSP Board of Adjustment must be submitted to the City prior to issuance of the Airport Zoning Permit and building permit.

**Fire Department Review Contact:** Laura McCarthy at lmccarthy@BloomingtonMN.gov, (952) 563-8965

**Public Works Review Contact:** Jen Desrude at jdesrude@BloomingtonMN.gov, (952) 563-4862

- 1) Update civil engineering plans in response to comments provided.
- 2) A Minnesota licensed civil engineer must design and sign all civil plans.

**Construction/Infrastructure Review Contact:** Jen Desrude at jdesrude@BloomingtonMN.gov, (952) 563-4862

- 1) Clarify--does this remove planting area include tree removal? There is no X like other tree removals so assuming that the tree remains
- 2) Clarify--remove, salvage and replace. Is it remove and replace with a new pole or salvage and reinstall the same pole?
- 3) Show curb and pavement removal for sanitary and watermain installation in sandy soil
- 4) Clarify--some of this pipe appears to be removed on utility plan
- 5) Modify irrigation system
- 6) Irrigation service was shot across the tracks without tracer wire. It is in this general vicinity
- 7) The utility plan still shows this hydrant in place
- 8) Is this the fill and abandon symbol? Remove instead
- 9) & wiring behind curb
- 10) Verify existing storm sewer layout.
- 11) Check if this light pole can remain. Based on utility plan it looks like it might need to be salvaged and reinstalled like the others to the south
- 12) Add sidewalk on west side of road
- 13) Why does this need to be depressed? Is this a fire access? Use B618 in public street--we don't want people driving into this area
- 14) Add sidewalk to south side of 82nd Street
- 15) Add Sidewalk
- 16)
- 17) Which way does this area drain? To the street? To this low area? Show the emergency overflow and that it drains before flooding the LRT tracks
- 18) FFE is listed at 816.5 but matching at 817? Is it ramped inside?
- 19) Show electrical and irrigation in blvd
- 20) Need 10' min separation between sanitary and watermain
- 21) Show isolation of watermain, monitoring and annodes due to proximity to LRT
- 22) See attached as-built sketches for watermain & valving

23)

24) The typical sections on C7.2 all seem to refer to D418 Special--this D418 detail appears standard. Only applicable for 82nd Street? Is it the color that makes it special or some type of notch out or something else? Suggest detailing it.

25) Suggest making the Select Granular Borrow as per Engineer's Direction. A lot of the surrounding area is sand that likely meets select requirements, although there are pockets of poorer material where this would be applicable.

26) Where is this? The two spots it might be on sheet 4.2 don't show reinforced turf, they both use concrete. No turf if this is near 30th Avenue. Would like to see B618 curb and gutter if it is at 30th Ave or some other type of protection to keep vehicles from driving in where they don't belong.

27) Don't see a call out for the replacement of the sidewalk along 30th Avenue---it is 5" thick

28) This will complicate future sanitary repairs. Suggest relocating power.

29) Create a section here to show, footing, water, sanitary, curb

30) Check light foundation and watermain for conflicts--suggest not placing light footings right on watermain

31) Note: ALL SHEETS - Check line type scale. Line type symbology isn't showing up, i.e. flow direction on sewer pipes

31) A Minnesota licensed civil engineer must design and sign all retaining walls 4-feet high and higher and setbacks for the principle structure must be provided.

32) Suggest moving the utility profiles closer to this utility plan instead of the C9.X series

32) Provide a sidewalk connection from the building to public sidewalk or street.

33) 12.5 is 3/4" aggregate as is B

9.5 is 1/2" aggregate as is A

This says two different things by being 12.5 and A mix

33) Illustrate that sidewalks meet ADA requirements for cross slope and longitudinal grade. Construct sidewalk ramps with metal truncated domes, if required.

34) SP 12.5 is 3/4" aggregate, but SPWEA is 1/2" aggregate.

They don't match

34) Restore City street by complying with the City Street Improvement Policy; contact Utilities (952-563-4568) for the requirements and show this on the plan.

35) Should this be the same as Inset A on the previous page? What size rock are you using in the mix? Don't want to drag rocks. Suggest switching to a 1.5" lift

36) Conflict between light pole and storm sewer

37) Check for conflicts between 42" existing pipe, new manhole and catch basin and angle of pipe to ensure adequate compaction and space can be provided.

38) North is to the right not up



- 39) Add note to maintain 8' minimum depth on watermain
- 40)
- 41) Adjust CB to match new grades
- 42) Too flat--recommend nothing less than 0.50% on bituminous pavement
- 43) Adjust CB Rim to accommodate new grading
- 44) Suggest showing the slope for the 11" hold down to ensure pedestrian and sidewalk plow safety.
- 45) Typical section shows 2%. The slope at this angle may be 2.31% but it isn't helpful since no one can recreate the slope direction in the field. Suggest deleting this and others on 82nd Street
- 46) This doesn't match the profile grade shown on Sht 8.0

**PW Admin Review Contact:** Jen Desrude at [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov), (952) 563-4862

- 1) A 10-foot sidewalk/bikeway easement shall be provided along all street frontages. Developer/owner shall provide the legal description and Engineering staff will prepare the easement document.
- 2) Public drainage/utility and easements must be provided on the plat as approved by the City Engineer.
- 3) Private common utility easement/agreement must be provided. Update existing agreement.
- 4) Private common driveway/access easement/agreement must be provided. Update existing agreement.
- 5) See list of items that must be included on the preliminary plat per Chapter 22 of the Bloomington City Code.
- 6) All development setbacks must be based on planned widened rights-of-way for the abutting public street(s) (Section 19.42)
- 7) The \$15 fee for Engineering staff to obtain a certified copy of the plat from Hennepin County must be provided prior to the plat being released for recording.

**Traffic Review Contact:** Jen Desrude at [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov), (952) 563-4862

- 1)
- 2) Furnish a construction traffic control plan. Plans shall be reviewed and approved by the City Engineer prior to the issuance of permits.
- 3) Illustrate on plan that the clear view triangle (15' from property corner to driveway approaches) is not obstructed by landscaping or signage.
- 4) Existing curb cuts must be removed and replaced with curb and gutter per City standard details.

- 5) Show and label existing street light poles, handholes, conduit, and signal interconnect on the plans. Street lighting and interconnect conduit must be exposed for city inspection prior to pouring concrete or backfilling excavation in city right-of-way.
- 6) Provide appropriate MMUTCD references for signs proposed for circulating traffic. All private signage must be installed outside of the city right-of-way.
- 7) All parking stall striping must be painted white. Parking islands must be 3-feet shorter than the parking stall and 8-feet wide.
- 8) Disabled parking signage and pavement markings must be placed in accordance with ADA and MMUTCD
- 9) List the number of parking spaces required by city code and the number of spaces provided on the site plan.
- 10) Modify note to read: Protect in-place light poles, and conduit and wiring between poles.
- 11) This is a street light handhole, not a pad. Please add note to protect in-place lighting handhole.
- 12) Both of these structures are handholes.
- 13) This is a traffic signal handhole that if removed or disturbed will require rewiring of a portion of the traffic signal system, including the interconnect wiring between LRT and traffic signal. Please schedule a meeting with Jen Desrude/Tom Bowlin to discuss what impacts will be occurring with this handhole.
- 14) Lighting handholes and poles along east side of 30th Avenue are shown but not the conduit. Please show the lighting conduits also and add notes to where the conduits will need to be protected.
- 15) Please show and label the signal system conduits, handholes and poles on the east side of 30th Avenue (south of LRT tracks).
- 16) Please show and label the signal system handholes, conduits and poles in this area.
- 17) Please show and label the lighting conduits in addition to the lighting handholes and poles.
- 18) Need to include a traffic control plan showing how this workzone will be developed or road closed. Include any proposed lane closures and signing out on and along East Old Shakopee Road.
- 18) Please include a haul routing plansheet. Please discuss with Tom Bowlin what proposed access locations for haul routing would be approved.
- 19) Please include a traffic control plan showing the following:
  - closure of the rightmost northbound thru lane along 30th Avenue (from approximately 82nd Street to LRT tracks)
  - closure of sidewalk along east side of 30th Avenue adjacent to site with pedestrian detour signing at LRT tracks and EOSR directing to west side sidewalk.
- 20) Do not depress curb in this area. Please show and label the traffic signal handhole and conduit infrastructure in this area so we can assess what impacts this project will have.

- 21) Reword note to read: Salvage and reinstall light pole. Remove existing and furnish and install new lighting wiring.
- 22) Suggest installing a gate at this location.
- 23) If grades allow, suggest extending sidewalk along the north lot curb line to connect 31st Street with ped bridge.
- 24) Add note: protect inplace traffic signal pole.
- 25) Please add notes indicating "salvage and reinstall" to any inplace signs along east side of 30th Avenue.
- 26) There can't be two thru lanes at this location. Please review and update text.
- 27) See notes elsewhere in submittal on the impacts of the utility work on the LRT and traffic signal system at Lindau Lane & 30th Avenue.
- 28) All construction and post-construction parking and storage of equipment and materials must be on-site. Use of public streets for private construction parking, loading/unloading, and storage will not be allowed.
- 29) All public sidewalks along East Old Shakopee Road shall not be obstructed
- 30) Storage of materials or equipment shall not be allowed on public streets or within public right-of-way.

**Utility Review Contact:** Jen Desrude at [jdesrude@BloomingtonMN.gov](mailto:jdesrude@BloomingtonMN.gov), (952) 563-4862

- 1)
- 2) Encroachment agreement application must be submitted for encroachment in public right-of-way or easements.
- 3) Utility as-builts must be provided prior to issuance of Certificate of Occupancy.
- 4) Use updated city standard details for driveways, utilities, erosion control, etc. found on the website at [www.bloomingtonmn.gov/information-sheets-and-handouts-engineering-division](http://www.bloomingtonmn.gov/information-sheets-and-handouts-engineering-division)
- 5) Move landscaping, pond, retaining wall, or other structure out of right-of-way or easement.
- 6) Utility permits are required for connections to the public storm, sanitary, and water system. Contact Utilities (952-563-8777) for permit information.
- 7) Contact Met Council (651-602-1378) for Sewer Availability Charge (SAC) determination.
- 8) A minimum 10-foot horizontal separation and 18-inch vertical separation is required between watermain and sewers.
- 9) Provide valves for system isolation (longest interval cannot exceed 400 feet) and for building isolation without shutting down supply to hydrants.
- 10) Install hydrants to provide fire protection for entire building. Each hydrant covers 150-foot radius. There are not enough hydrants at the Health Partners building. And another hydrant should be added on the SE side of the proposed parking ramp.  
Can Bloomington Fire get to the proposed hydrants on the north side of the ramp? There must be a hydrant located within 50 feet of all building fire dept connection points.

- 11) Provide a minimum of 8-feet and a maximum of 10-feet of cover over all water lines, valves, services, etc.
- 12) Use Class 52 DIP water main for pipe 12-inches in diameter and smaller. A minimum 8 mil polywrap is required on all DIP.
- 13) Minnesota Department of Health (MDH) water permit/review may be required. Provide a copy of MDH approval letter or written confirmation from MDH that no permit/approval is required.
- 14) Minnesota Pollution Control Agency (MPCA) sanitary sewer permit/review may be required. Provide a copy of MPCA approval letter or written confirmation from MPCA that no permit/approval is required.
- 15) An inspection manhole is required on all commercial sewer services.
- 16) Use standard short cone manholes without steps.
- 17) Install interior chimney seals on all sanitary sewer manholes.
- 18) Taps of live water mains are done by City forces and paid for and coordinated with the Contractor.
- 19) Utility and mechanical contractors must coordinate the installation of all water and sewer service pipes into the building to accommodate city inspection and testing.
- 20) Sanitary sewer mainline, clean-outs, manholes, and services must be designed with adequate depth of cover or install high-density polystyrene insulation to prevent freezing.
- 21) Use schedule 40, SDR 26, or better for PVC sewer services.
- 22) Combination fire and domestic services must terminate with a thread on flange or an MJ to flange adapter.
- 23) All components of the water system, up to the water meter or fire service equipment must utilize protective internal coatings meeting current ANSI/AWWA standards for cement mortar lining or special coatings. The use of unlined or uncoated pipe is not allowed.
- 24) Watermains and Hydrants can be no closer than 20 feet from LRT tracks - To protect the City water system from stray current corrosion.
- 25) All levels of the parking ramp that have sidewalls more than 50% open must drain to the storm sewer.
- 26) Suggest installing the future sewer crossing under the LRT tracks (at 31st St) now to avoid having to disrupt the areas south of the tracks in the future.
- 27) Install new fire hydrants instead of moving old ones as the old ones often don't pass hydrostatic testing without excessive extra effort and time.
- 28) Please check the City's water and sewer GIS records against the plans as some valves and and pipe sizes are not shown correctly. Several changes were redlined on the plans.
- 29) Can Bloomington Fire Get Access to these Hydrants

**Water Resources Review Contact:** Steve Segar at ssegar@BloomingtonMN.gov, (952) 563-4533

- 1) Provide stormwater management plan meeting the requirements of Bloomington Comprehensive Surface Water Management Plan. Currently under review.
- 2) A National Pollutant Discharge Elimination System (NPDES) construction site permit and Storm Water Pollution Prevention Plan (SWPPP) must be provided.
- 3) An erosion control bond is required. Dollar amount based on area disturbed:
  - \* 0.00-0.50 acres disturbed - \$5,000
  - \* 0.51-0.75 acres disturbed - \$8,000
  - \* 0.76-1.00 acres disturbed - \$11,000
  - \* over 1 acre disturbed - \$0.25 per square foot rounded to the nearest \$1,000 (maximum \$25,000)
- 4) Show erosion control BMP locations on the plan. Use bloomington details. No bales allowed for inlet protection. All materials must meet MNDOT approved materials list. Use approved inlet protection at all active storm sewer inlets; only baset or sack style in traffic areas. Include a turf establishment plan.
- 5) List erosion control maintenance notes on the plan.
- 6) HDPE pipe connections into all concrete structures must be made with water tight materials utilizing an A-Lok or WaterStop gasket or boot, cast-in-place rubber boot, or approved equal. Where the alignment precludes the use of the above approved watertight methods, Conseal 231 WaterStop sealant, or approved equal will only be allowed as approved by the Engineer.
- 7) Utility permits are required for connections to the public storm, sanitary, and water system. Contact Utilities (952-563-8777) for permit information.
- 8) Consider meeting to discuss Stormwater Management Plan updates and plans. Contact Bryan Gruidl (952-563-4557, bgruidl@BloomingtonMN.gov) to set up.
- 9) label storm sewer
- 10) Provide storm system details.
- 11) Provide profiles for CB leads within the right-of-way.

**Assessing Review Contact:** Kent Smith at ksmith@BloomingtonMN.gov, (952) 563-4975

- 1) With no expansion to the building area occurring in connection with the new plat, there will not be a park dedication fee required.